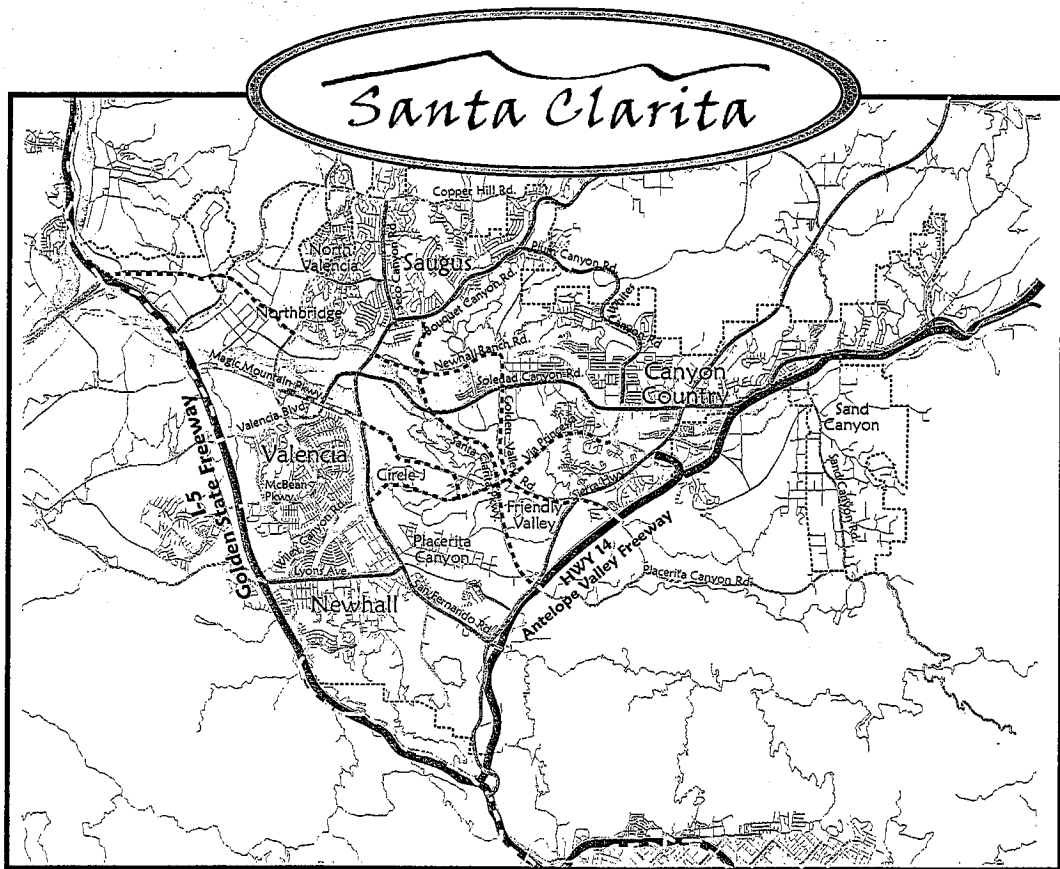


SANTA CLARITA BEAUTIFICATION MASTER PLAN



DECEMBER 11 - 2001

Santa Clarita Beautification Master Plan

Adopted by City Council
December 11, 2001

Prepared for:
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Executive Summary

The Beautification Master Plan is a tool to aid the City of Santa Clarita in accomplishing their long-term goal of citywide streetscape improvements and beautification. It addresses concepts for streetscape design, landscape enhancement, gateways, and monumentation and signage on both a regional and a community scale. Integrating these concepts, along with the issues of continuity, longevity, eliminating blight, and maintenance, will provide the City with a comprehensive and unifying plan for future streetscape corridor beautification. The Master Plan strives to maintain the identity of the individual communities while unifying the whole of Santa Clarita.

The Beautification Master Plan is divided into three main parts:

1. Citywide Guidelines: A general framework of elements that unify the City as a whole. Examples listed for signage are preliminary in nature. A sign designer will be contracted for ultimate design of all signs at the time of implementation.
2. Individual Community Guidelines: More detailed guidelines with specific community-oriented recommendations.
3. Implementation: An outline of estimated costs, phasing and priorities of the beautification improvements.

The Master Plan reflects the input of both community residents throughout the City of Santa Clarita and various City staff in the Planning, Parks and Recreation, Engineering and Field Services departments. Obtaining their input through public workshops and community meetings, and understanding their self image and visual preferences, was an important step in developing beautification goals. In addition, the Master Plan reinforces the goals of the Community Design Element of the General Plan which prioritizes the preservation of and improvement to the general physical aspects and amenities of the community including architecture, landscaping, roadways, open spaces and views, and the overall image of the City in relationship to its surroundings.

Through the development of citywide guidelines creating a unifying framework to include a monumentation program, gateways, signage and streetscape improvements, Santa Clarita will benefit from an enhanced regional identity. This citywide identity will tie in closely with the identities of the four individual communities of Valencia, Saugus, Newhall, and Canyon Country. The continuity of elements between these two levels of improvements is what gives the Master Plan strength and aesthetic integrity for the City as a whole.

A high level of craftsmanship and material quality is emphasized to uphold "value engineering" goals and ensure the longevity and lasting aesthetic appeal of the project. Using materials of high quality will help reduce maintenance and lifetime costs of the project. This dedication to "value engineering" will be discussed further in the implementation section of this document.

CITYWIDE

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I. Citywide Design Guidelines

A. Introduction

1. Citywide Identity

Santa Clarita is nestled between the Santa Susana and San Gabriel mountain ranges. With the mountains as a backdrop, Santa Clarita is a beautiful and thriving city with a suburban and rural small town feeling. Incorporated December 15, 1987, the City continues to grow and offer an attractive family setting, a beautiful landscape, a low crime rate and a strong education system, all within close proximity to Los Angeles. With Los Angeles only minutes away, Santa Clarita is a balanced blend of urban sophistication and a rural ranch character. Though one city, Santa Clarita is comprised of four main communities: Saugus, Valencia, Newhall, and Canyon Country. Each one of these individual communities has its own identity and character. While it is important to create a "citywide" identity, it is equally important to preserve and enhance the individual community identities.

Rich with history and natural elements such as rivers, mountains, valleys, heritage oak trees and Bouquet Canyon stone, Santa Clarita has much to draw from when defining its identity. Tree-lined streets, landscaped medians, and ranch-style fencing begin to create an identifiable character in parts of town, but they could be strengthened to become powerful unifying elements to unite the City as a whole. Establishing an object vocabulary of easily recognizable streetscape elements and signage throughout Santa Clarita will help develop a citywide identity. These same elements can have variations that also embrace the individual identities of the four communities within. Repeating these elements, forms, colors and landscaping will be the key to successfully creating and reinforcing a citywide identity.

B. Citywide Signage Program

Development of a citywide public signage program is a key element in establishing visual unity throughout the City of Santa Clarita. It will assist in unifying the separate communities into an easily identifiable, cohesive city. The program provides a set of design parameters to guide the design and redesign of public signage along the City's public rights-of-way. Such signage includes roadway directional signage, street signs and other non-regulatory signage. This does not include any traffic control signs. All signage shall be reviewed and approved with the appropriate agency. Business signage that is owned and maintained by individual property owners is private signage and is regulated by the Unified Development Code, Section 17.19. The purpose of the public signage program is to convey a clear design image that helps create a unifying theme throughout the City. The sign program will include the following elements:

- Gateways ~ Identification and design treatment of all primary and secondary City gateways;
- Monuments ~ Creation of gateway, directional and open space monuments which help to reinforce City identity;
- Street signs ~ Roadway approach and street signs;
- Special event signs and banners; and
- Consolidation of City public signs.

Basic design recommendations will promote design creativity and variation while insuring consistency in materials, form, scale, proportion and pedestrian orientation. Public signs within the City of Santa Clarita should meet the following basic requirements:

- Signs should be simple and easy to read.
- Signs should have a unified graphic theme.
- The City-developed logo, crest, or icon should be displayed on signs and gateway elements (icon design will be refined at the implementation level).

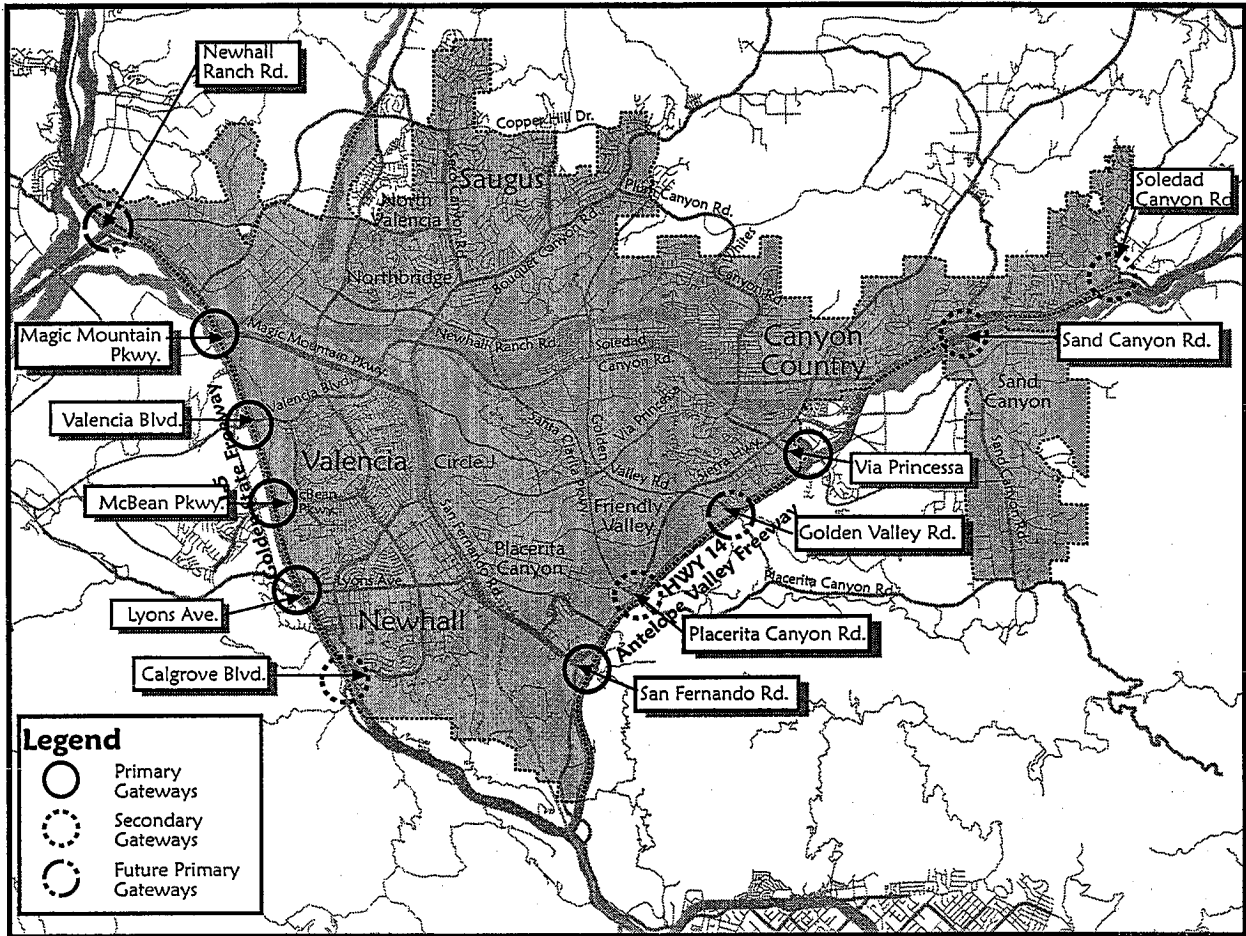


City Icon Example

1. Gateways

Gateways are intended to identify primary entrances into the City of Santa Clarita with "gateway scenes" that, 1) signify entry into the City, and 2) enhance and announce the identity of Santa Clarita as a whole. The composition of elements and materials in the gateway monumentation should function as a visual anchor. The specific design may vary at different gateways; however, the elements of the design scene should be repeated. Using these elements as a standard, a system of monumentation and markers throughout the City will be repeated at smaller sign locations. To the extent that this *repetition* brings *recognition* - our goal is achieved.

- The gateway designs should reflect natural elements and materials found within the Santa Clarita Valley and the surrounding area.
- The gateways should be of significant scale to clearly identify these areas as major City entries and be easily legible to motorists.
- Gateway implementation should be constructed in the first phase of the Master Plan implementation.



Regional Gateways Map

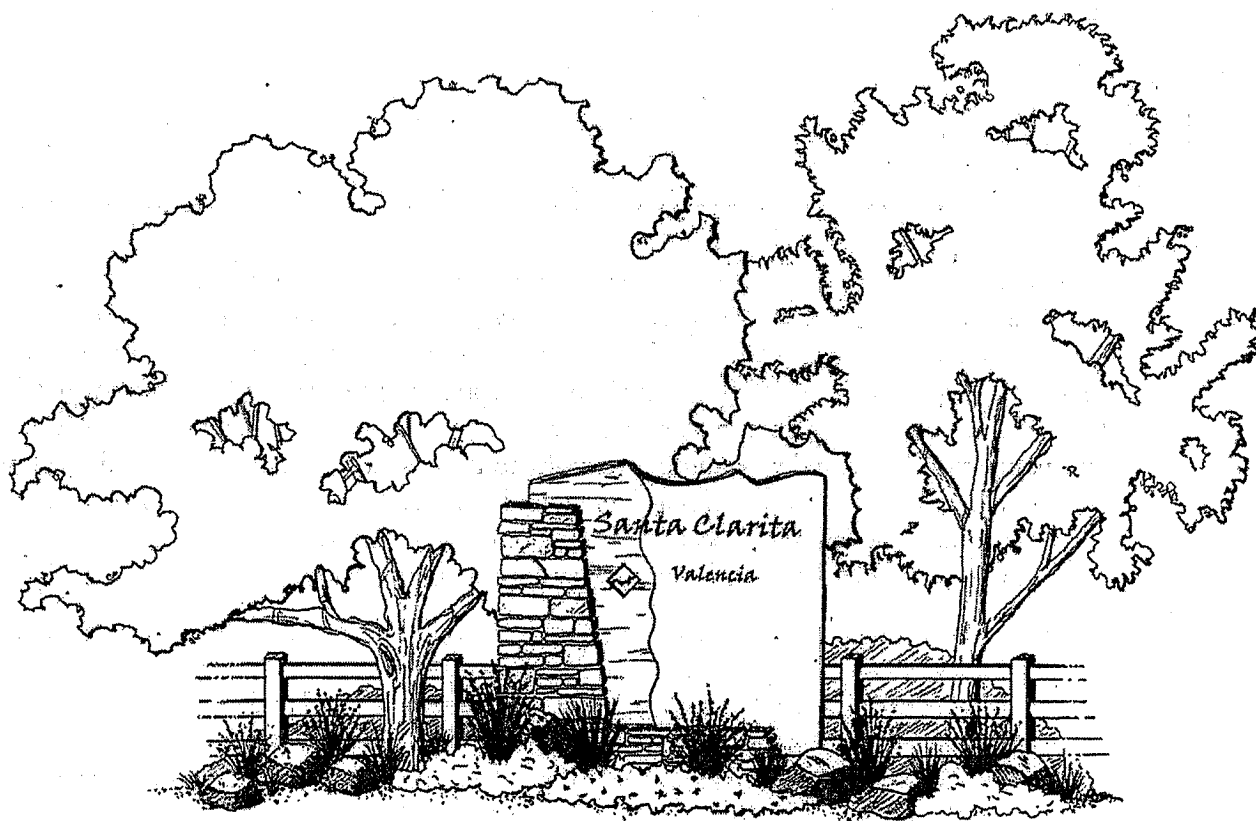
The following gateway locations have been identified in the map above and have been categorized as primary and secondary gateways. During various public workshops conducted with members of the community, it was determined that ‘primary’ gateways are those that are used most by residents. Those that have been designated as secondary gateways are highly used, but lesser traveled locations. The designation of a gateway location is dependent on the volume of traffic and population living and working in proximity to these areas. As part of future development additional right-of-way dedication may be required to meet the objectives. Gateway designs and improvements at the following locations should be coordinated with the California Department of Transportation (Caltrans) or private landowners as appropriate:

- Primary Locations at I-5 Freeway Off-ramps: Lyons Avenue, McBean Parkway, Valencia Boulevard, and Magic Mountain Parkway
- Primary Locations at SR14 Off-ramps: San Fernando Road and Via Princessa
- Secondary Location at I-5 Freeway Off-ramp: Calgrove Boulevard
- Secondary Locations at SR14 Off-ramps: Placerita Canyon Road, Sand Canyon Road, and Soledad Canyon Road
- Future Primary Location at I-5 Freeway Off-ramp: Newhall Ranch Road
- Future Primary Location at SR14 Off-ramp: Golden Valley Road

Gateway Scene:

Though gateway scenes may vary slightly, they should incorporate the same combination of elements, both architectural and landscape, to reinforce the concepts of City identity, City entry, and unified design. These elements should include:

- 1) An entry monument sign;
- 2) Bouquet Canyon stone;
- 3) A three-rail fence (refer to the *Citywide Landscaping Palettes* section);
- 4) Heritage Live Oaks and Cottonwoods; and
- 5) Plants selected from the *Citywide Landscaping Palettes* section.

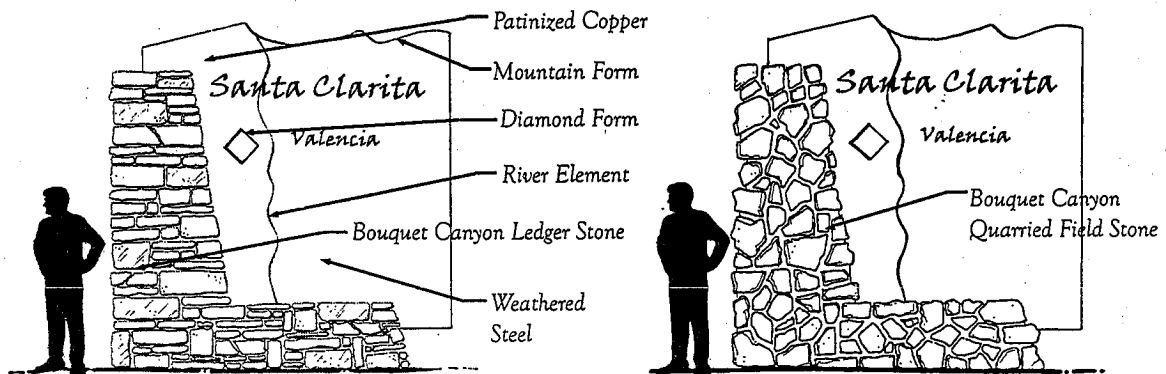


Example of a Gateway Scene

Entry Monument:

As a key element of the gateway scene, entry monument signs clearly identify arrival into the City. Entry monuments should have the following characteristics:

- Entry monument signs will be located at each of the previously mentioned gateway locations and should be coordinated with Caltrans or private landowners as appropriate.
- Signs should be lighted and landscaped.
- Monument signs should be an integral part of the gateway scene that includes an entry monument sign, a fence, and plants and trees selected from the *Citywide Landscaping Palettes* section of the document.
- Signs should be well proportioned and accented with landscaping. The scale of the signs should be visible from freeways. Planting at maturity must not obscure safe vehicular sight lines.
- Signs should incorporate complementary colors, materials, and lettering fonts. More than one material should be used in the construction of the sign.
- The monument signs should incorporate a combination of urban and natural materials that reflect a blend of urban sophistication with a rural ranch character. The materials should include: a weathered steel form (Cortin steel), Bouquet Canyon stone, and a copper element with patina.
- The same materials, color scheme, and graphic theme should be used on all entry monument signs. This will make it very clear to visitors and residents alike that the signs have been placed as an effort to unify the City. The City icon should appear on each sign and the font should be replicated exactly. The mountain form above “Santa Clarita”, which has been depicted with a specific font, captures the essence of the City icon.
- The diamond form is a consistent element found throughout the signage program. The opening provides character in the entry monument design as well as a strong link to the street sign design.



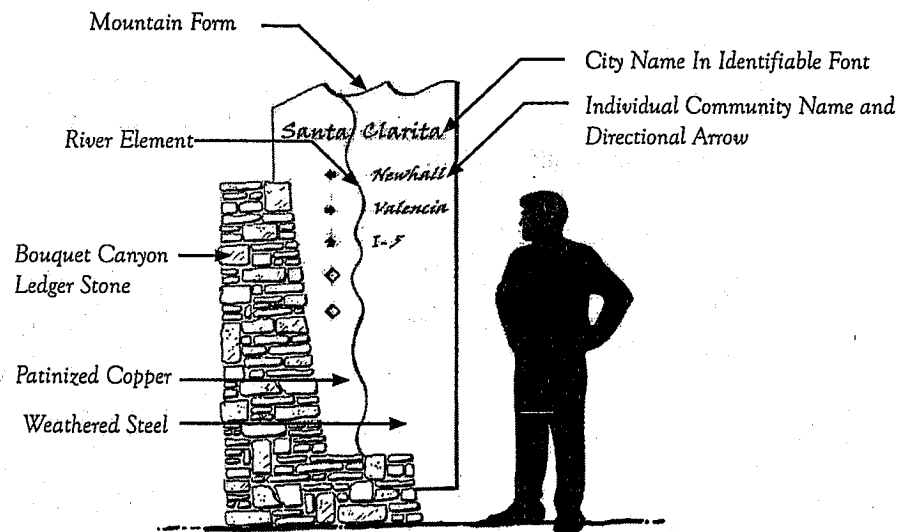
Entry Monument Sign Example - Option 1

Entry Monument Sign Example - Option 2
(All materials are the same as Option 1)

2. Directional Monuments

Directional monument signs are smaller versions of the entry monument signs that indicate how to find specific destinations within the City. They provide clarification and enhance the sense of entry to a specific community from the major arterials and City limits. Directional monuments shall have the following characteristics:

- Directional signs will be located at key intersections and should be coordinated with private landowners as appropriate.
- Signs should be lighted, landscaped and prominently placed to increase visibility for motorists.
- Signs should be well proportioned and accented with landscaping. The sign scale should be in proportion to adjacent buildings and landscaped areas. Plantings at maturity must not obscure safe vehicular sight lines.
- Signs should incorporate complementary colors, materials and lettering fonts. More than one material should be used in the construction of the sign (see example below).
- Directional signs should be constructed of the same materials as the monument signs. Signs should incorporate a rustic combination of native, organic and natural materials that reflect the ranch and rural character of the City gateways. The materials should include: a weathered steel form; Bouquet Canyon stone; and may include a patinaed copper or metal element.
- All directional signs should use the same material, color scheme and graphic theme. The Santa Clarita icon should appear on each sign and the font should be replicated exactly.

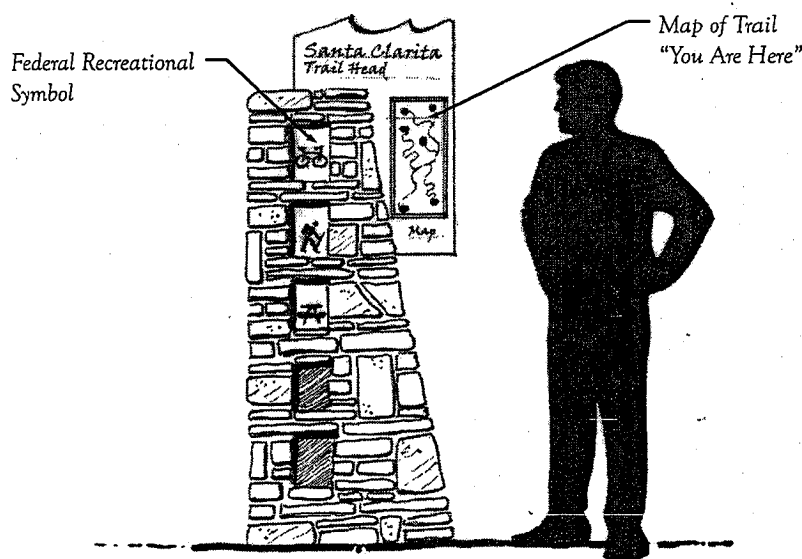


Directional Monument Sign Example

3. Open Space Trailhead Monuments

Open space trailhead monuments enhance the sense of arrival at key points of interest. Santa Clarita has an extensive bicycle and pedestrian trail system. Placement of trailhead markers at key access points will assist in reinforcing a citywide image. Open space trailhead monuments shall have the following characteristics:

- Open space trailhead monuments will be located at bicycle and pedestrian trailheads, park entrances, river access points and at pedestrian bridges.
- Signs should be lighted, landscaped, and prominently placed so as to be highly visible to pedestrians, equestrians, and bicyclists.
- Signs should be well proportioned and accented with landscaping. The sign scale should be in proportion to adjacent buildings and landscaped areas.
- Signs should incorporate complementary colors, materials and lettering fonts. More than one material should be used in the construction of the sign.
- Open space trailhead monuments should be constructed using at least one of the key materials used in the monument signs (a weathered steel form, Bouquet Canyon stone, or a shiny copper element with a patina). Signs should incorporate a rustic combination of native, organic and natural materials that reflect the ranch and rural character of the City gateways. Additional materials may include concrete or wood.
- All open space trailhead monuments should use the same material, color scheme, and graphic theme. The Santa Clarita icon should appear on each sign and the font should be replicated exactly.



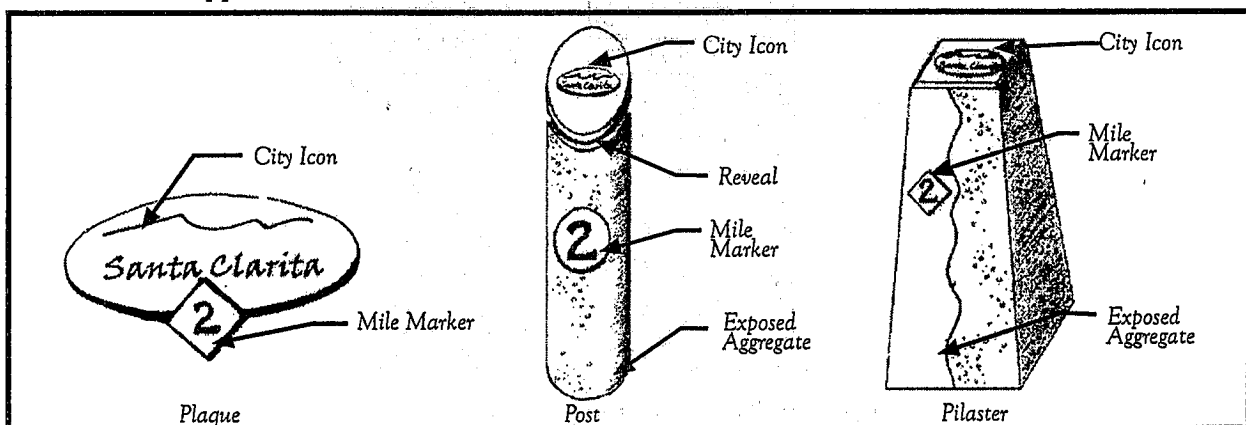
Open Space Trailhead Monument Example
(Materials and Forms Reflect Gateway Monument Signs)

4. Trail / Open Space Markers

Trail and open space markers will be located along bicycle and pedestrian trails at one-mile increments. The open space trailhead monuments (described on the previous page) welcome users while entering the trail system; the trail and open space markers will measure how far users have traveled while visually linking the City as one cohesive unit. Pedestrians and bicyclists will have a sense of comfort in identifying their location along the trail. Repeating forms and elements provide users with a sense of place and location, while unifying the trail system and the City. The open space and trail “marker” is intended to give a feeling of comfort and familiarity, while at the same time enabling a walker, jogger or bicyclist to know how far they have traveled. These markers may take many forms including a plaque placed on a pedestrian bridge or bench, or a small monument. There are a number of alternatives available to achieve this repetitive feature. Three examples are shown below: a plaque; a post; and a pilaster. The plaque could be set in existing paving or mounted to bridges, signs or other permanent trail features. The post and pilaster should be installed adjacent to trails and should be linked to the trailhead monuments.

Trail and open space markers shall have the following characteristics:

- Open space / trail markers will be located on bicycle, equestrian and pedestrian trails.
- Markers should be spaced one per mile, contain a mile marker indicating the number of miles traveled and be linked to the open space trailhead monument signs.
- Markers should be prominently placed so as to be easily visible to equestrians, pedestrians and bicyclists.
- Markers should be constructed using a rustic combination of native, organic and natural materials that reflect the character of the City gateways. The proposed markers shown below are formed from concrete with a portion of exposed aggregate.
- The Santa Clarita icon should appear on each marker and the font should be replicated exactly.
- Posts and pilasters should be approximately 3'-6" high and 10" to 12" in diameter.
- Homeowner Associations in communities with special standards district may present an alternative trail sign design to be used in their district for the City’s review and approval.



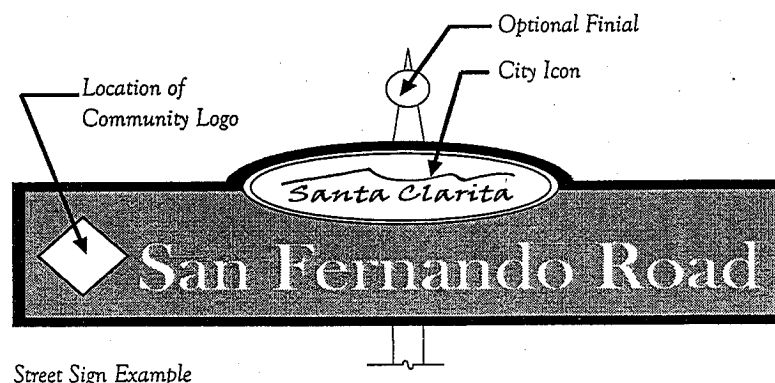
Trail / Open Space Marker Examples

5. Street Signs

Street signs provide one of the strongest unifying elements in the signage program. Every street within Santa Clarita will be identified in the same way to provide a recognizable sense of repetition that clearly enables a motorist, bicyclist or pedestrian to know where they are and if they are still within the City limits. A unique color for the sign, the font selection, a finial (if incorporated), and the use of the City icon are all important aspects of the sign and its ability to unify the City. In addition, a diamond containing a different color logo for each of the four communities will establish the uniqueness of each individual community without reducing the impact of a citywide sign design.

All street signs should have the following characteristics:

- The sign color palette should be the same for all street signs (except for community logo).
- All street signs should be the same size and shape.
- The same font type and size should be selected for each sign.
- A finial could be included on each sign.
- The City icon should be an integral part of the sign design to reinforce a unifying theme.
- A community logo should be incorporated as shown below. Refer to the *Sign and Logo* section of each individual *Community Design Guidelines*.
- All street signs should be designed to meet ADA standards for visibility compliance.

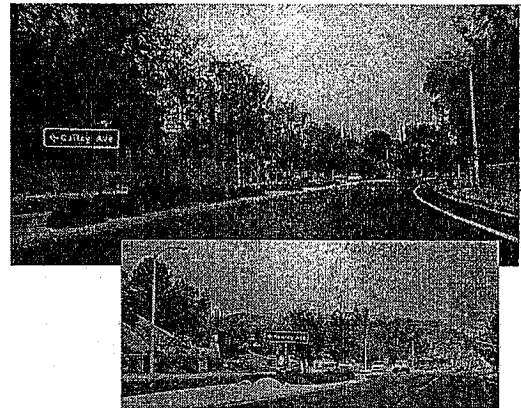


6. Roadway Approach Street Signs

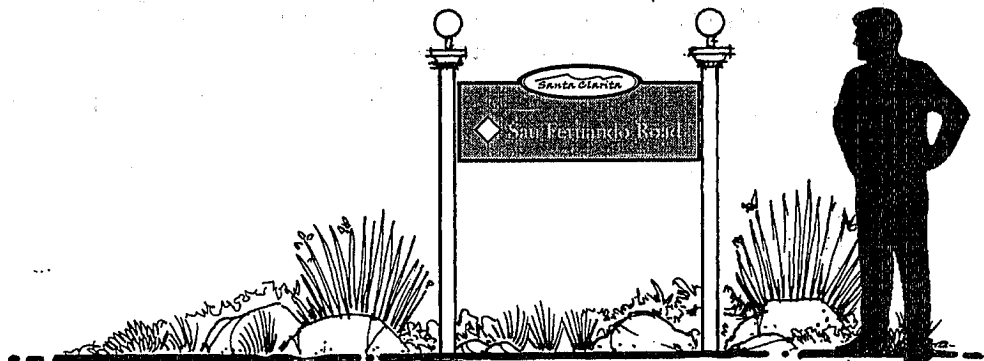
Roadway approach street signs are often located on medians to notify motorists of the upcoming street so they may turn in a safe and timely manner. These signs should be designed with the same elements used in the traditional street signs to repeat and reinforce the unifying sign theme.

All roadway approach street signs should have the following characteristics:

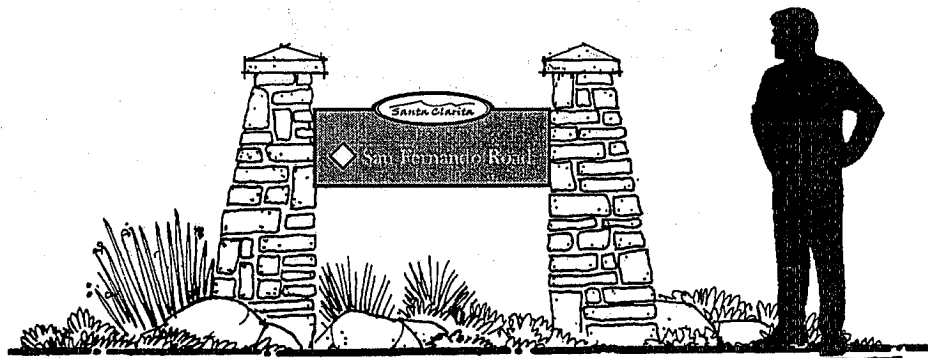
- The sign color palette should be the same as that for street signs.
- All roadway approach street signs should be the same size and shape.
- The same font type and size should be selected for each roadway approach sign.
- The City icon should be an integral part of the sign design in order to unify the City.
- A community logo should be incorporated as shown below. Refer to the *Sign and Logo* section of each individual *Community Design Guidelines*.
- All street signs should be designed to meet ADA standards for visibility compliance.



Typical Roadway Approach Street Signs



Roadway Approach Sign - Option 1



Roadway Approach Sign - Option 2

7. Special Event/Seasonal Banners

Special event/seasonal banners are temporary signs permitted in the public right-of-way. Banners are allowed on approved street light standards and approved paseo bridges as a means of publicizing events sponsored or cosponsored by a public or nonprofit agency. Special events are defined in the City's Sign Ordinance as "a unique happening, action, purpose or occasion," such as, the Santa Clarita Marathon and the Cowboy Poetry and Music Festival. The City's Sign Ordinance limits the display duration to 21 days per City event and requires that no more than two 3' x 8' banners be located on any given light standard and no more than 75 light standards be used for each event. In addition, no more than one 4' x 30' banner can be located on any given paseo bridge.

In addition, special event/seasonal banners should have the following characteristics:

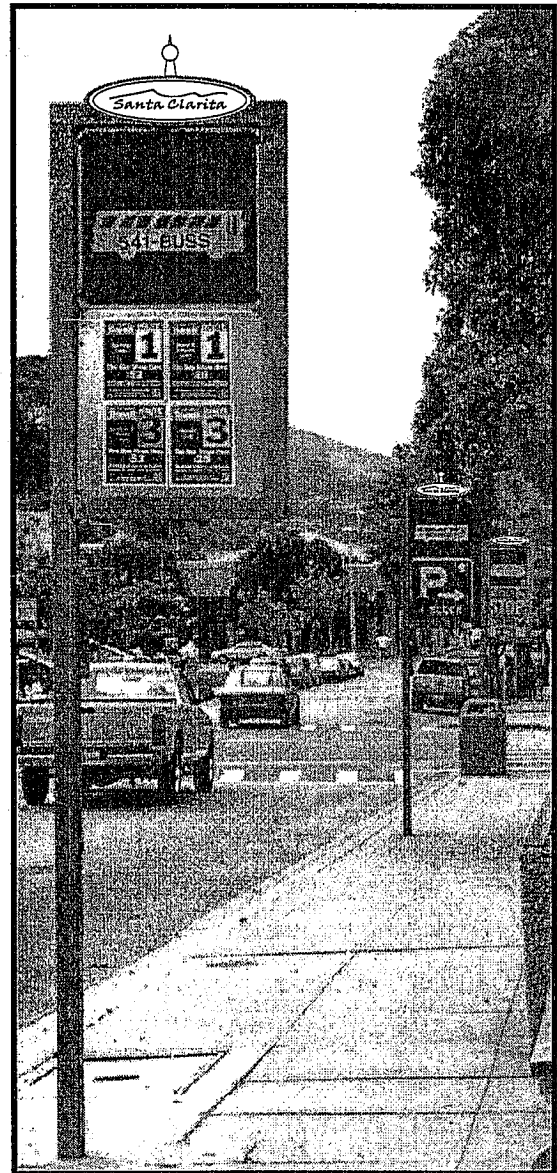
- The lowest point of the banner must be a minimum of 17' above the roadway. This height will accommodate large trucks traveling along the street.
- If light poles have not been engineered to accommodate banners (additional wind loads), utilize banners with break-away arm supports.
- Signs should be safe, neat, and compatible with the area.
- Lettering should be clear, precise, and simple with minimal graphics to avoid distracting motorists and creating traffic hazards.
- When appropriate, the City icon should be an integral part of the sign design to reinforce the *Citywide Signage Program* and the City's image.



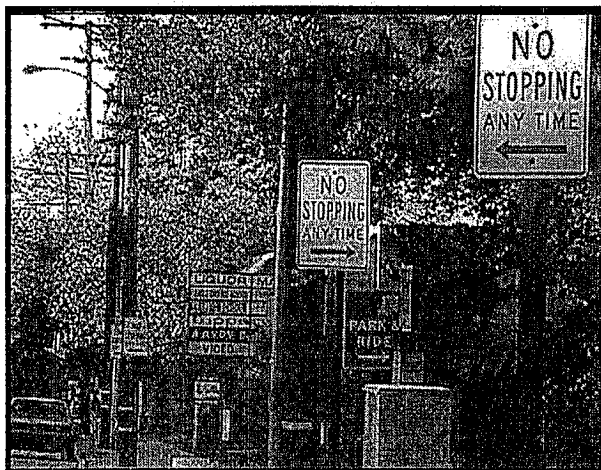
Special Event Banner Example

8. Consolidation of Public Signage

Regulatory, directional, and traffic signs should be consolidated in order to reduce roadway clutter, simplify and enhance the streetscape, and aesthetically improve the City. A single pole should be provided with a standard space sized-to-fit interchangeable signs. Bus stop signage and "No Parking" signs should be consolidated in combination signs where practical. Aside from street signs, combination signs are the most common type of sign used, and consequently act as a strong unifying element if designed properly. The City icon should be incorporated into the sign design and the structural components of the sign post should be painted the same color. In areas where several directional, transit and vehicular signs are present, the signs should be grouped together in a designed sign structure which can be replicated throughout the City. This will help to reinforce the overall theme of signage as a unifying element for the City of Santa Clarita.



Combination Signs Reduce Sign Clutter and Enhance Streetscape

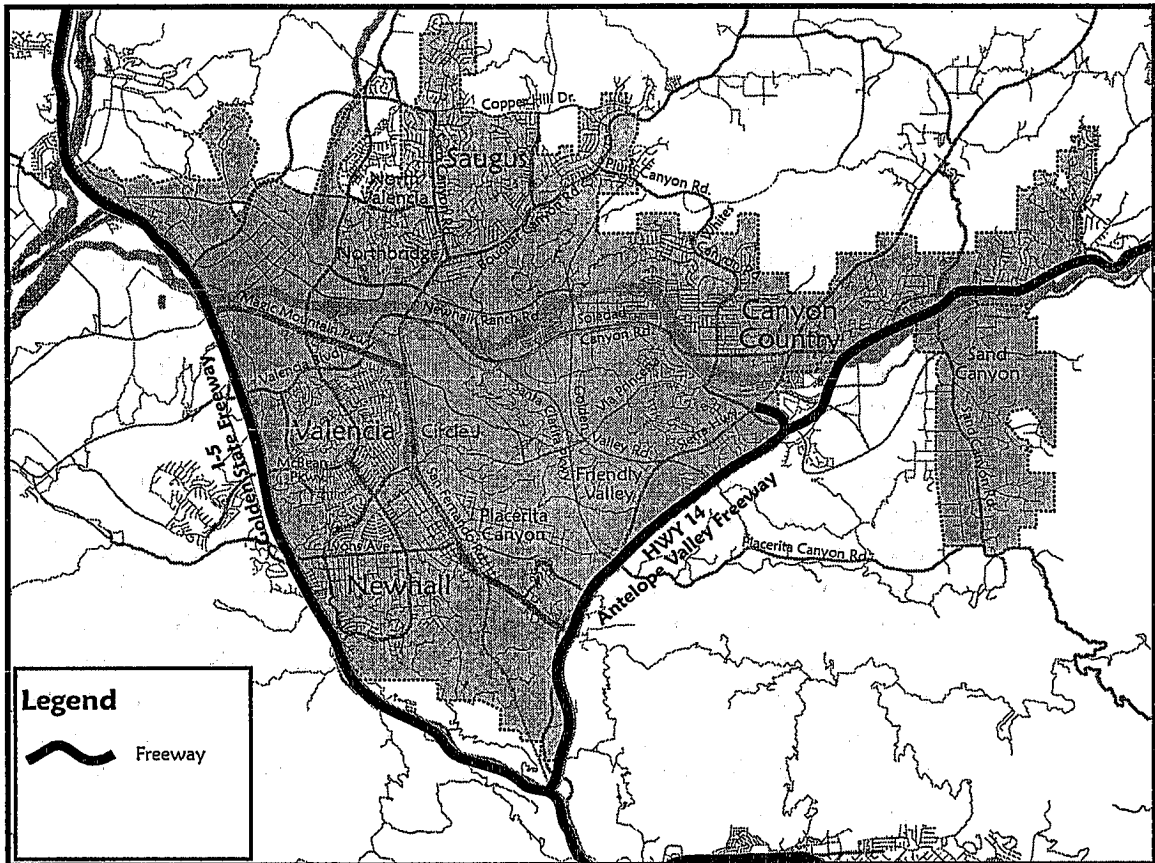


Example of Sign Clutter (San Fernando Road)

9. Commercial / Business Signage

Although prohibited along the City's rights-of-way, privately-owned business signage is a prominent feature along any corridor with a mix of commercial, office, dining and service establishments. This can result in a hodgepodge of signage ranging in size, shape, height, color and physical condition. These signs can be distracting to motorists, have a negative aesthetic impact and affect the overall character of the public space. Goal 8 of the City's General Plan Community Design Element directs the City "To ensure that signage throughout the City is visually attractive and minimizes distraction." Ongoing enforcement of the City's Sign Ordinance advances this goal and the City's effort to eliminate nonconforming signage citywide.

In general, it is the City's desire to minimize the number of pylon signs in favor of more pedestrian-scale monument signage. For businesses with 100' of street frontage, a six-foot-high monument sign should be used as the primary means of on-street identification. For shopping centers with three acres of land and 500' of lineal street frontage, two monument signs, rather than one pylon sign, should be utilized. Business signage should be aesthetically complementary to the structures they advertise. In situations where structures are old or dilapidated and new signage is proposed, signage should be designed to influence the overall appearance of the facility. Improvements to private business signage can be the catalyst for improvement to other on-site features, such as the building façade or parking area.



Regional Freeway Map

C. Streetscape Beautification

Streetscapes throughout Santa Clarita provide a thread of continuity that, when beautified, can unify the City and delineate each community. Areas that provide an opportunity for beautification are freeways, freeway on- and off-ramps, roadway medians, parkways and street furniture along major streets and cross-town connections, and primary or key intersections. With such a vast area encompassed by the City, vehicular thoroughfares have become the most used element to connect areas within each community, and one community to another.

1. Freeways

The above map delineates the roadways within or bordering Santa Clarita that are designated as "freeways." These include Interstate 5 (Golden State Freeway, I-5), and State Route 14 (Antelope Valley Freeway, SR14). Though these are both state highways and under the jurisdiction of the California Department of Transportation (Caltrans), beautification guidelines included in this Master Plan encourage the City to work with Caltrans on beautifying these corridors. These corridors are particularly important since they create the first impression of the area as motorists enter or drive by the City of Santa Clarita and serve as two-thirds of the City's borders, as shown on the map above. Beautifying these freeway corridors with landscaping, fences, and gateway monuments will help create an identifiable character that unifies and enhances Santa Clarita.

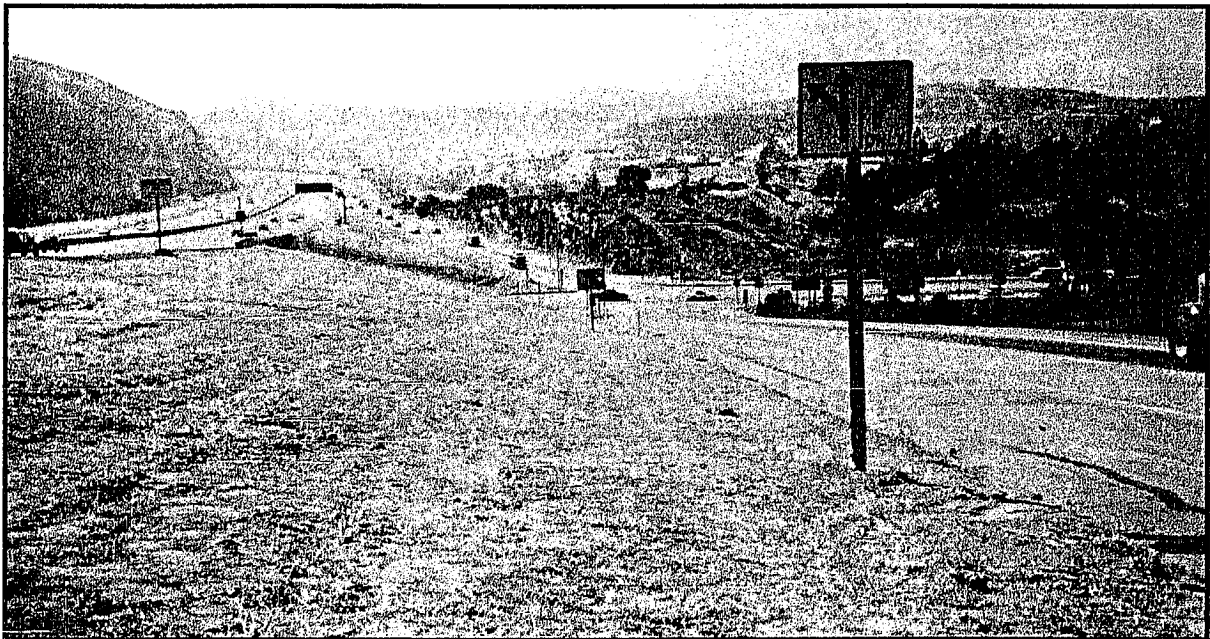
2. Freeway On- and Off-Ramp Design

Priority should be given to landscaping Santa Clarita's primary entrances and interchange areas. Freeway on- and off-ramps are typically the first and last impressions a motorist receives as they enter and exit the City, thus special consideration should be taken regarding their design. As motorists enter the City, they will experience a progression of beautification improvements beginning with freeway landscaping, then progressing to gateway scenes at the freeway on- and off-ramps and reinforced at major intersections.

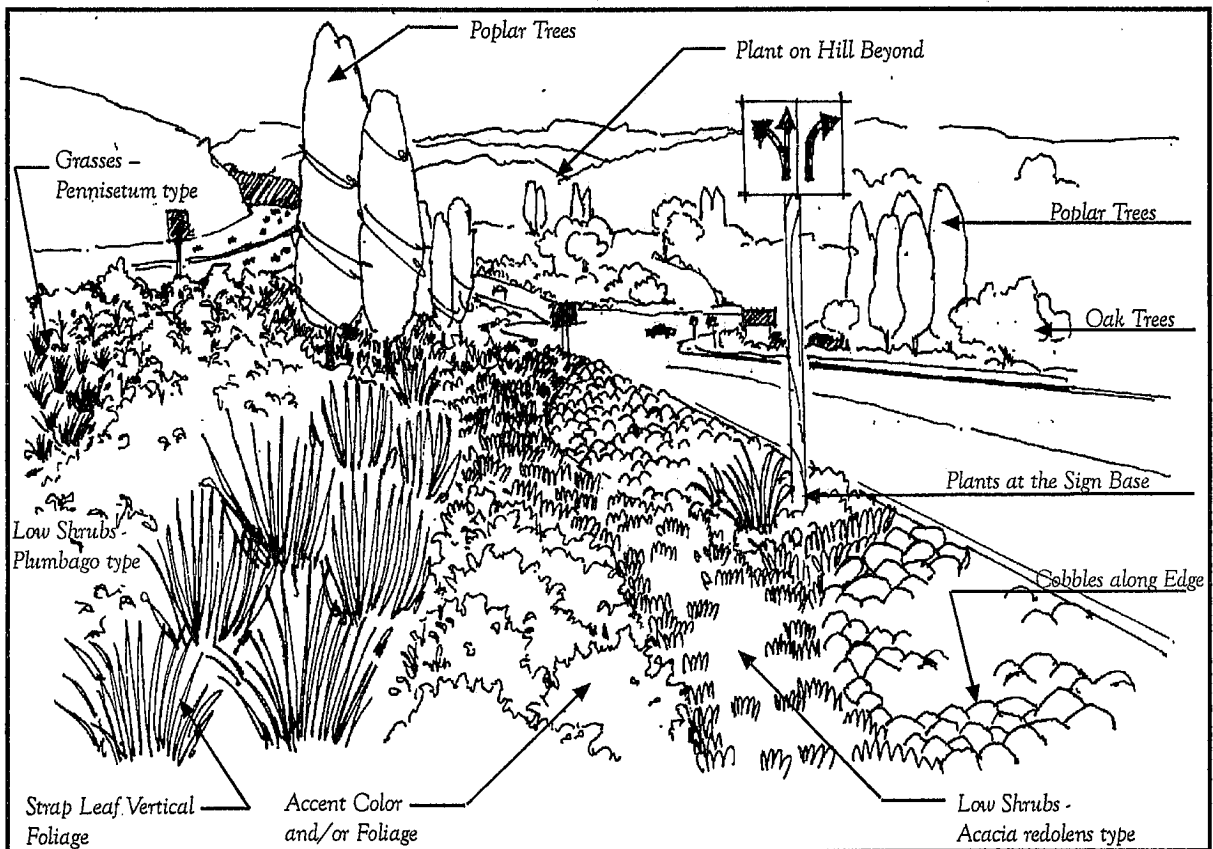
Freeway on- and off-ramp design shall incorporate the following:

- Landscaping along the freeway on- and off-ramps within the public right-of-way is encouraged. Such projects will need to be coordinated through Caltrans, however, this will create a visually appealing, as well as aesthetically enhancing, experience for motorists passing along these corridors.
- Off-ramps at primary gateways must incorporate all of the elements described in the *Gateways* section.
- The use of Mediterranean and drought-tolerant plants in conjunction with an efficient and long-term water system should be utilized for the establishment and maintenance of a drought-tolerant landscape.
- Trees and shrubs should be located and spaced to allow for mature and long-term growth. Trees and shrubs should be carefully located to avoid site distance conflicts.
- Materials may include stone, filter fabric to reduce weed growth, and boulders at strategic placement.

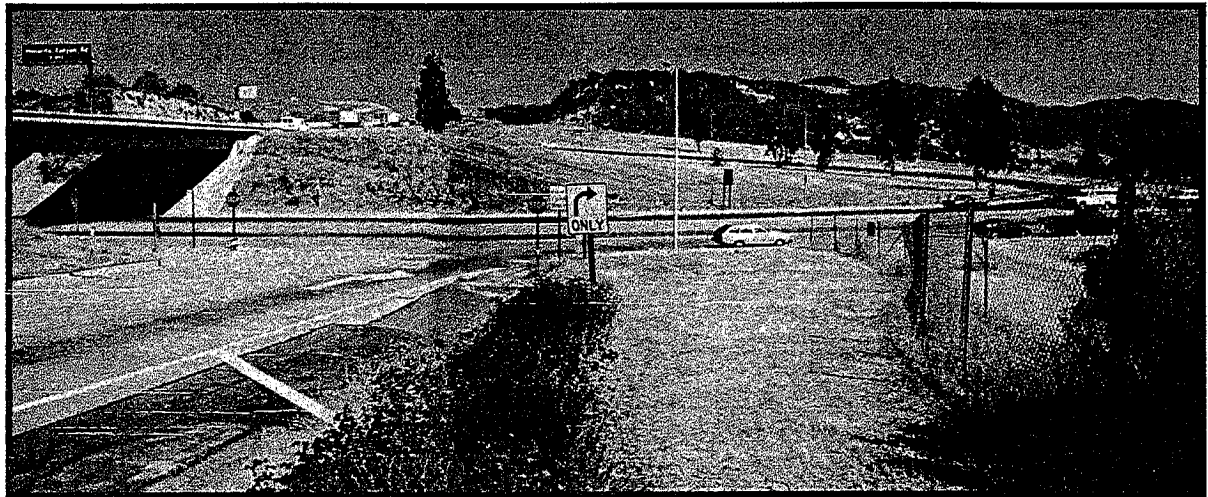
The following illustrations demonstrate the progression of beautification improvements that could take place at freeway on- and off-ramps, gateways and major intersections.



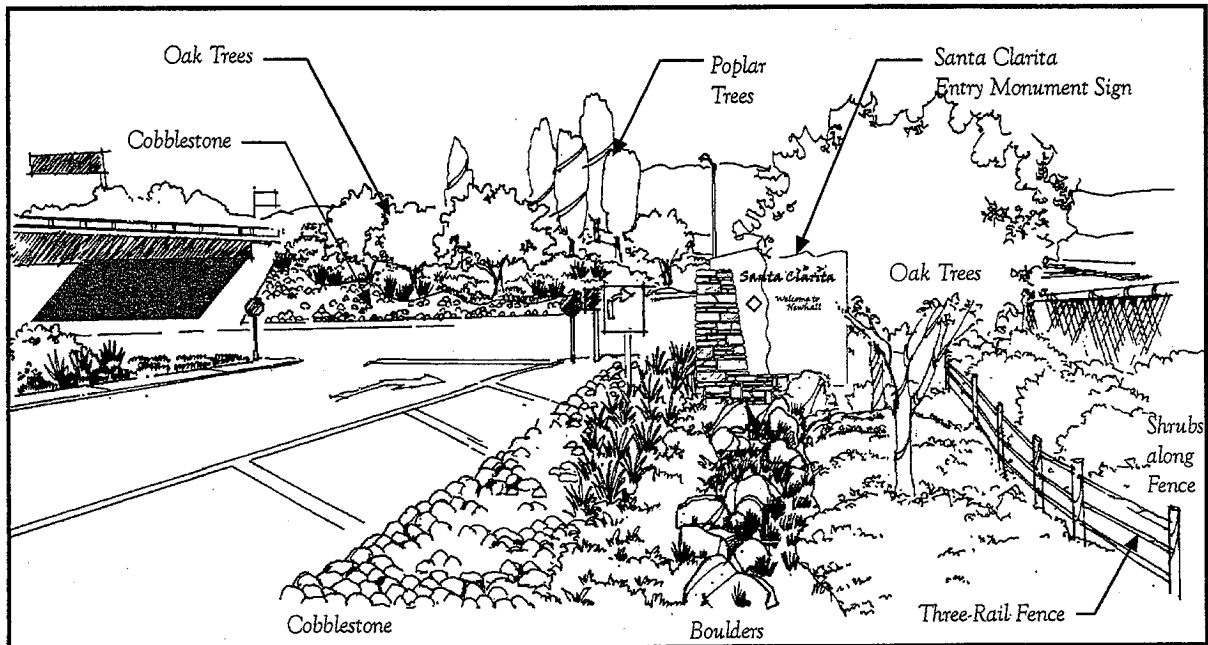
Before: State Route 14 at the San Fernando Road South Off-Ramp



After: State Route 14 at the San Fernando Road South Off-ramp with Improvements



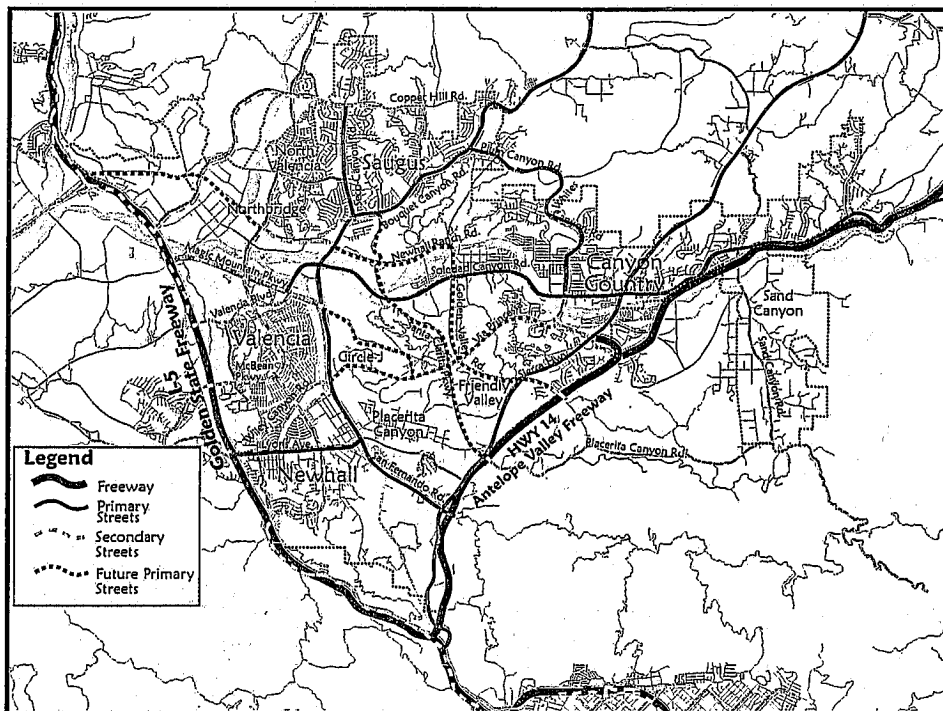
Before: State Route 14 at the San Fernando Road North Off-ramp



After: State Route 14 at the San Fernando Road North Off-ramp with Gateway Scene

3. Primary Corridors

It is the goal of this Master Plan to set forth long-range guidelines for the improvement, enhancement and beautification of Santa Clarita's vehicular corridors. These include freeways and primary corridors that have impacts, on a regional scale, to the aesthetic appeal of the City. During various public workshops conducted with members of the community, it was determined that the corridors that have been designated as primary are those that are traveled most by residents. The designation of a primary corridor is dependent on the volume of traffic and population living and working in proximity to these areas, however the locations were determined without conducting traffic studies. Corridors designated as first priority should be improved prior to those designated as second priority. This process will be discussed

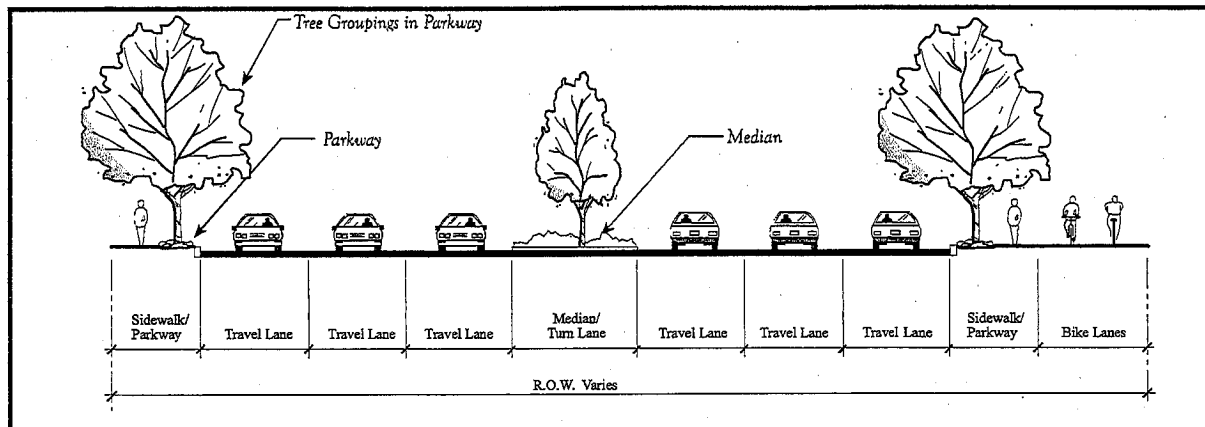


Primary Corridors Map

further in the *Implementation* section of this document. Each individual primary corridor design will vary depending on the community it extends through, but the use of a consistent palette of key elements used in the beautification of all primary corridors will unify the City as a whole. For example, all primary corridors will consist of a landscaped parkway, landscaped median designs, fencing/walls and selected street furnishings.

The following is a list of primary corridors:

- First Priority Primary Corridors: Lyons Avenue, Seco Canyon Road, San Fernando Road, Soledad Canyon Road, Plum Canyon Road / Whites Canyon Road, Bouquet Canyon Road, Sierra Highway and Valencia Boulevard (between Magic Mountain Parkway and Bouquet Canyon Road).
- Second Priority Primary Corridors: Valencia Boulevard (between I-5 and Magic Mountain Parkway), Via Princessa, Sand Canyon Road, Placerita Canyon Road, Copper Hill Road, McBean Parkway, Rye Canyon Road, Valley Street / Calgrove Boulevard, The Old Road, Wiley Canyon Road, Haskell Canyon Road, Magic Mountain Parkway and Orchard Village Road.
- Future Primary Corridors: Golden Valley Road, Newhall Ranch Road and Santa Clarita Parkway.



Example of Roadway Section

Parkways:

Each parkway will include a palette of trees, shrubs, ground covers, vines to screen walls and fences or additional walls where space is available. Landscaping that is both attractive and functional will strengthen the overall appeal of these roadway corridors. The spacing of proposed landscape planting should recognize the primarily vehicular function of these thoroughfares. Also, carefully planned landscape planting, along with the proposed median planting, will promote a ‘green corridor’ look, helping these roadways incorporate more of the natural environment found in outlying undeveloped areas. The landscaped parkways are strongly encouraged in commercial and industrial areas, as well as residential areas to create roadways that are aesthetically pleasing and pedestrian friendly.

Parkways should have the following characteristics:

- Street trees should be planted a minimum of 50 feet on center to mitigate traffic noise and add to the aesthetics of the street and create a “green” corridor.
- The City’s Unified Development Code (UDC) requires a five-foot parkway to be planted between the curb and the sidewalk in all new residential developments.
- Trees should not be planted any closer than 10 feet from any driveway and should include 15-gallon size species staked per City standards.
- The general tree character should be compatible with adjacent existing street trees and, if possible, provide a large canopy of shade for pedestrians and bicyclists.
- Street trees should incorporate the following characteristics: drought tolerance; minimal fruit drop; non-invasive root systems; and high canopies (seven feet above grade).

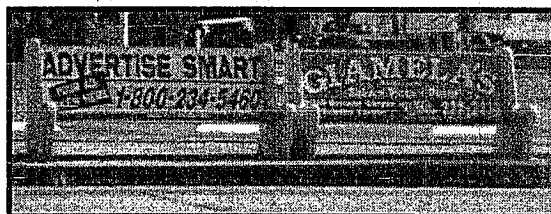
Refer to the Citywide Landscaping Palettes for a list of recommended trees and landscaping to be used along specific corridors. All landscaping shall comply with City Ordinance 90-15 and is subject to review and approval by the Director of Parks, Recreation and Community Services.

Tree Grates:

In areas where pedestrian traffic is prominent, tree grates should be implemented to enlarge the walkable surface area. Wider sidewalks allow for people traveling in opposite directions to pass without disrupting the flow of travel. Tree grates allow for more usable shaded sidewalk and for tree plantings even in areas that do not allow much space for landscaping.

Street Furniture:

Due to the vehicular nature of these roadway corridors, new street furniture will be limited to bus and transit locations. As a direct result of the public outreach process, it is strongly recommended that advertising be eliminated from all public benches. This kind of advertising, when spread throughout the City, has a tendency to erode the aesthetic integrity of



Advertising on Bus Benches

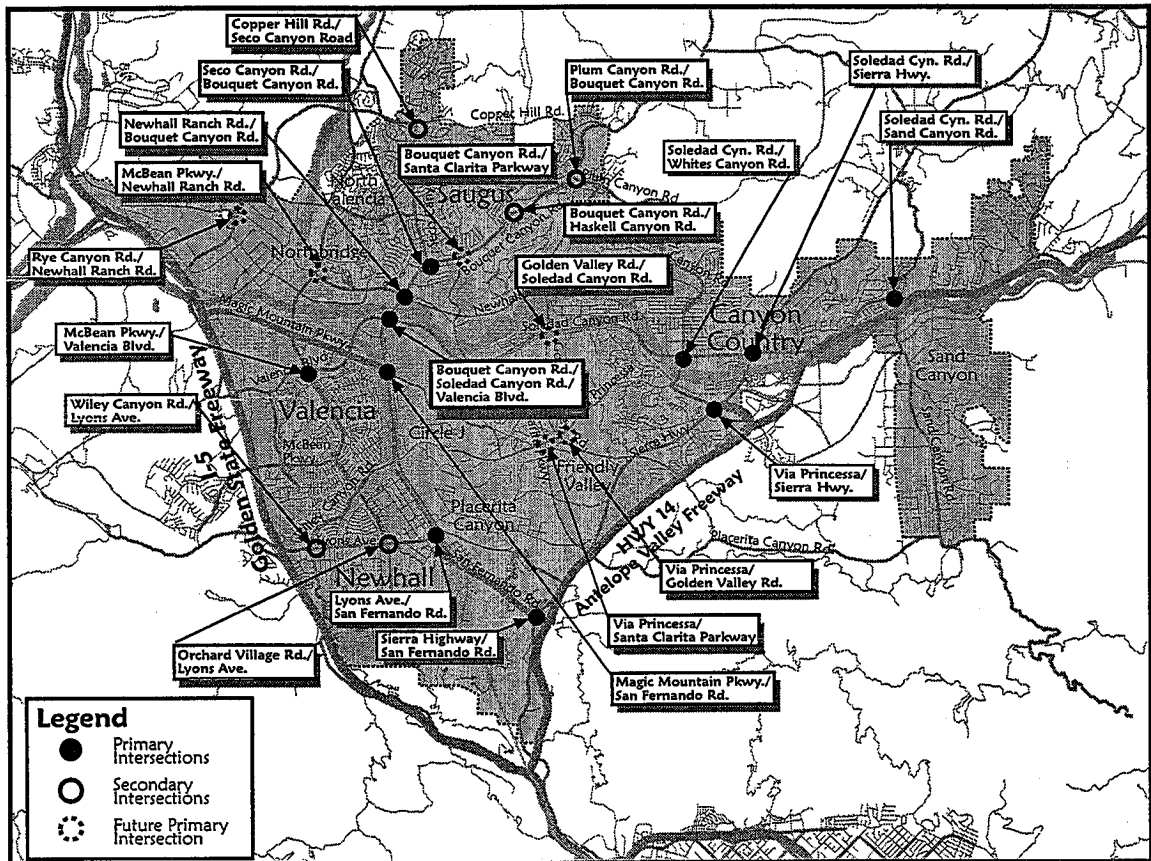
any proposed beautification efforts. Advertising in bus shelters is also discouraged, but, if unavoidable, should be restricted to a special frame which is integrated as part of the shelter. Refer to the *Citywide Public Transportation* section for additional information.

Fences / Walls:

Fences and walls will be used to unify the City of Santa Clarita. They provide accent, help define a space and can be used to buffer parking lots adjacent to the roadway. In addition, fences used at vacant lots can “bridge” large void spaces and provide unity along the street scene. Each community will incorporate its own fencing type at intersections and along primary corridors where appropriate. Refer to the *Community Design Guidelines* for additional information on individual communities.

Pedestrian and Bicycle System:

The City currently has several trail systems. The six basic types are the Class I bike trail, Class II bike lanes, Class III bike routes, equestrian trails, hiking trails and paseos. A pedestrian and bicycle system has been incorporated with some connections to existing trail systems. Bicycle storage and locking areas should be located in key areas throughout the City. The public rights-of-way accommodate pedestrian-oriented sidewalks. Large through-streets have incorporated bicycle lanes to encourage recreational and convenience cycling.



Key Intersections Map

4. Intersections

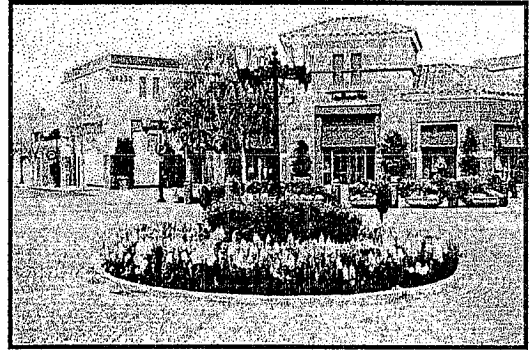
Intersections, because of their high significance involving vehicular and pedestrian interaction, play a vital role in corridor beautification. Due to their function as traffic controlling devices and pedestrian circulation nodes, they potentially have the highest visual impact on passing or stopped motorists. Intersections also provide the greatest opportunity to implement a citywide signage program, visually enhance the roadway corridors with landscape plantings and provide pedestrian-friendly circulation components. Refer to the *Community Design Guidelines* for specific intersection treatments and designs within each community. The following is a list of key intersections as identified by community representatives:

- Primary Intersections: Seco Canyon Road and Bouquet Canyon Road, Newhall Ranch Road and Bouquet Canyon Road, Soledad Canyon Road and Bouquet Canyon Road, McBean Parkway and Valencia Boulevard, Magic Mountain Parkway and San Fernando Road, Lyons Avenue and San Fernando Road, Sierra Highway and San Fernando Road, Whites Canyon Road and Soledad Canyon Road, Sierra Highway and Soledad Canyon Road, Soledad Canyon Road and Sand Canyon Road, and Via Princessa and Sierra Highway.
- Future Primary Intersections: Santa Clarita Parkway and Bouquet Canyon Road, Santa Clarita Parkway and Via Princessa, Golden Valley Road and Via Princessa, Newhall Ranch Road and McBean Parkway, and Newhall Ranch Road and Rye Canyon Road.

- Secondary Intersections: Seco Canyon Road and Copper Hill Road, Plum Canyon Road and Bouquet Canyon Road, Bouquet Canyon Road and Haskell Canyon Road, Wiley Canyon Road and Lyons Avenue, and Orchard Village Road / Valley Street and Lyons Avenue.

Accent Paving:

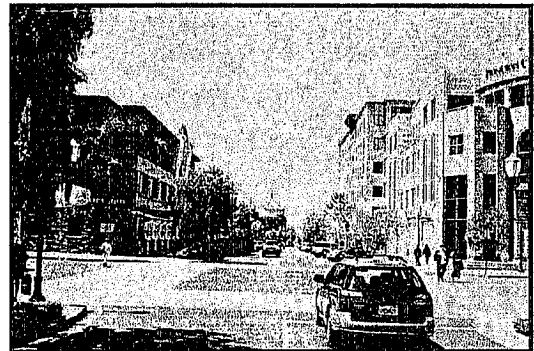
The use of accent paving provides an opportunity to both dress up the intersection and incorporate traffic calming measures for pedestrian and bicyclist safety. The change in texture is a visual and intuitive signal to both the motorist and pedestrian of the appropriate boundaries for each. Accent paving contributes greatly to the overall visual appeal of the intersection and is typically located in one or more of the following areas: at corners; on sidewalks; at pedestrian crossings; and in the middle of intersections. The texture should be smooth enough so that an older person, cyclist and persons with disabilities are not deterred.



Example of Accent Paving

Pedestrian Crossings and Intersections:

Incorporating special paving at pedestrian crossings, along with intersections, is an excellent way to visually cue the oncoming motorist to slow down because of possible pedestrian traffic. The texture of special paving in the crossings is another tangible cue for motorists to reduce their speed and pay increased attention to their surroundings. Special paving also enhances the pedestrian experience, both in visual appeal and safety, as pedestrians travel across various intersections.



Example of Special Paving at Intersection with Pedestrian Crossings

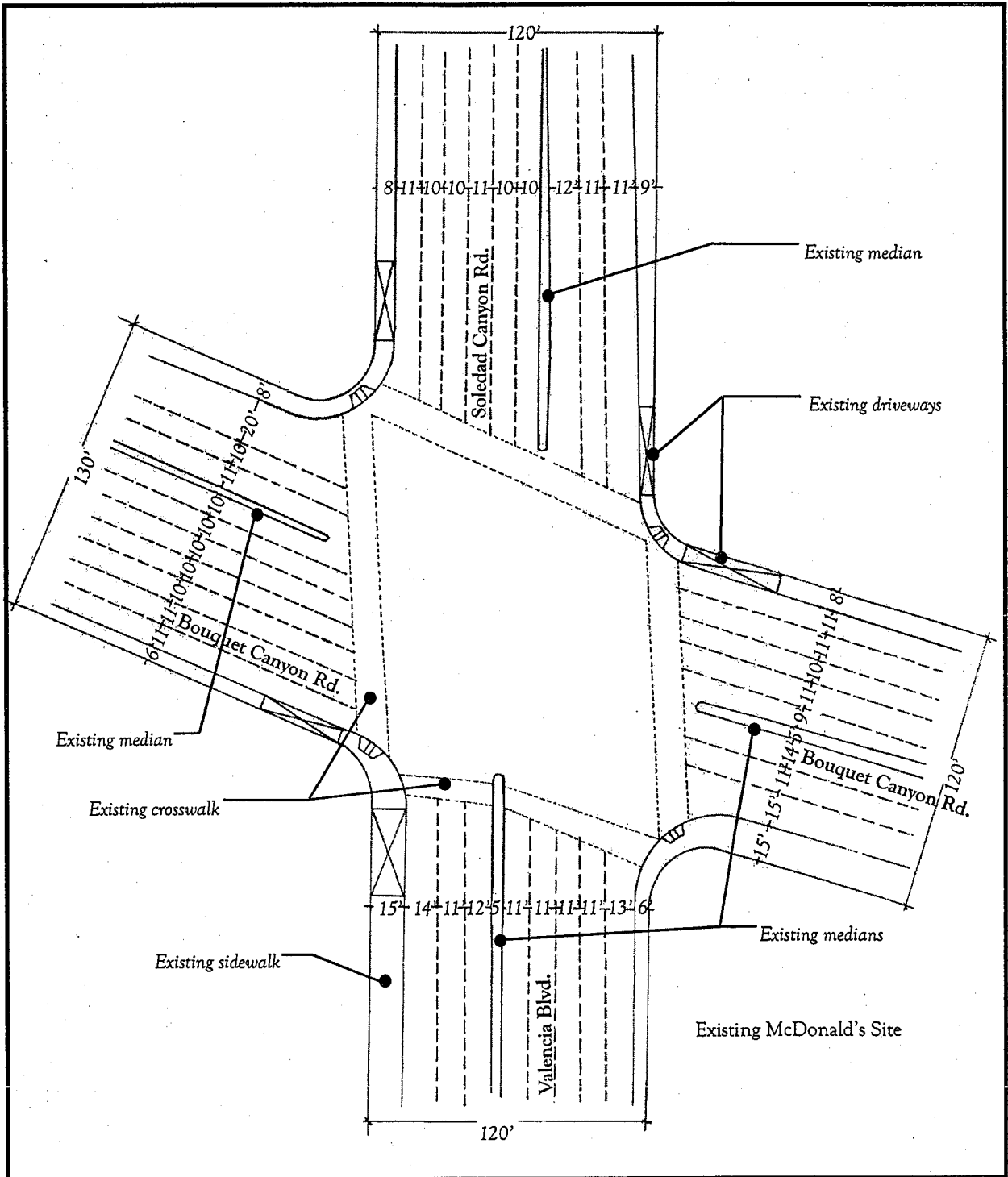
Corner Treatments:

Intersection corners often provide opportunities for beautification improvements. Adding low walls, special paving and landscaping at intersection corners will help buffer parking lots and service station lots, while adding color and continuity to intersections.

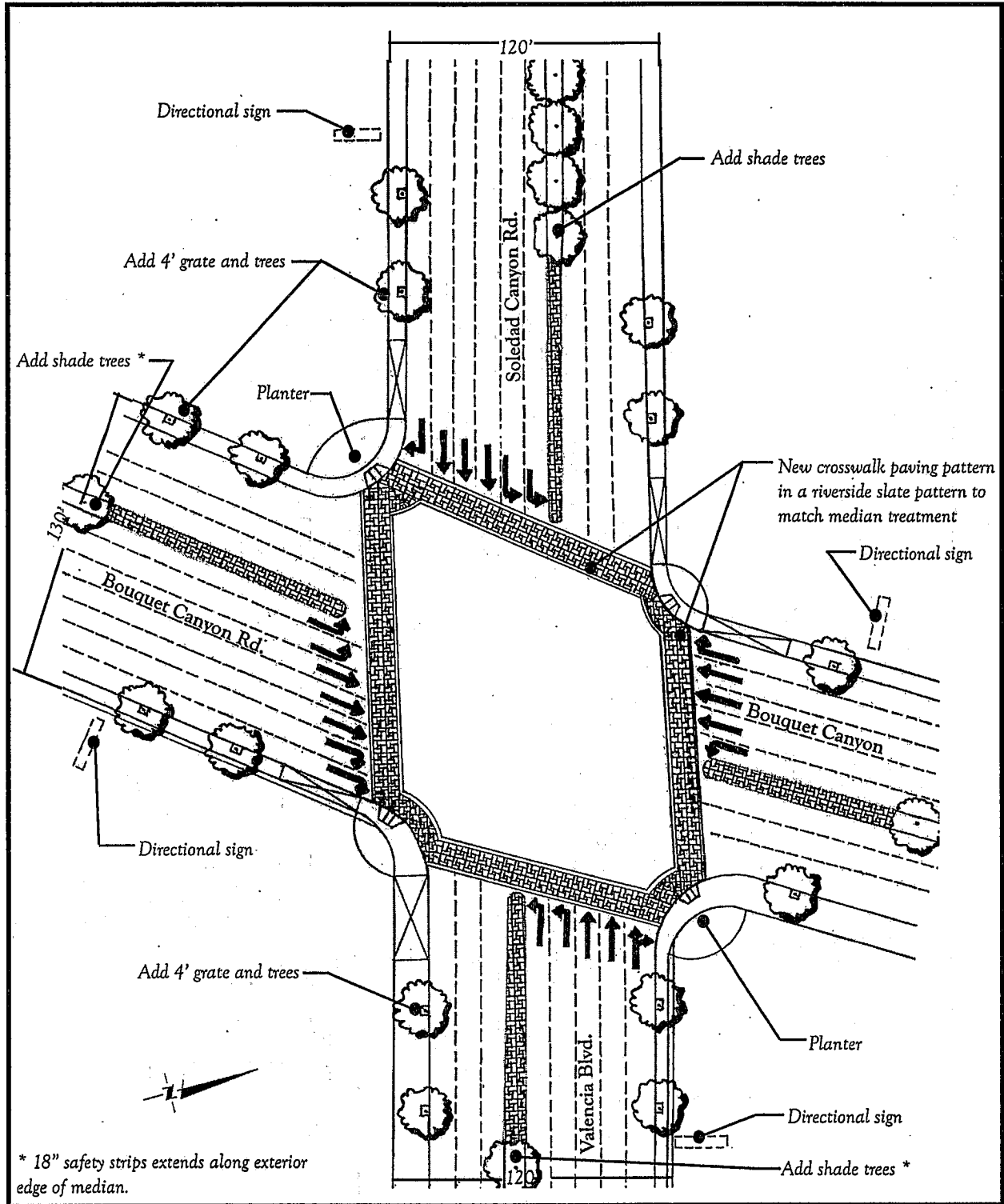
Intersection corners and medians are also a great place to implement the citywide signage program (Refer to the *Citywide Signage Program* section). Monument signs not only take advantage of increased pedestrian and slowed/stopped vehicular traffic, but also provide directional and locational information and increase the overall aesthetic appeal of the intersection.

Example of Intersection Improvements:

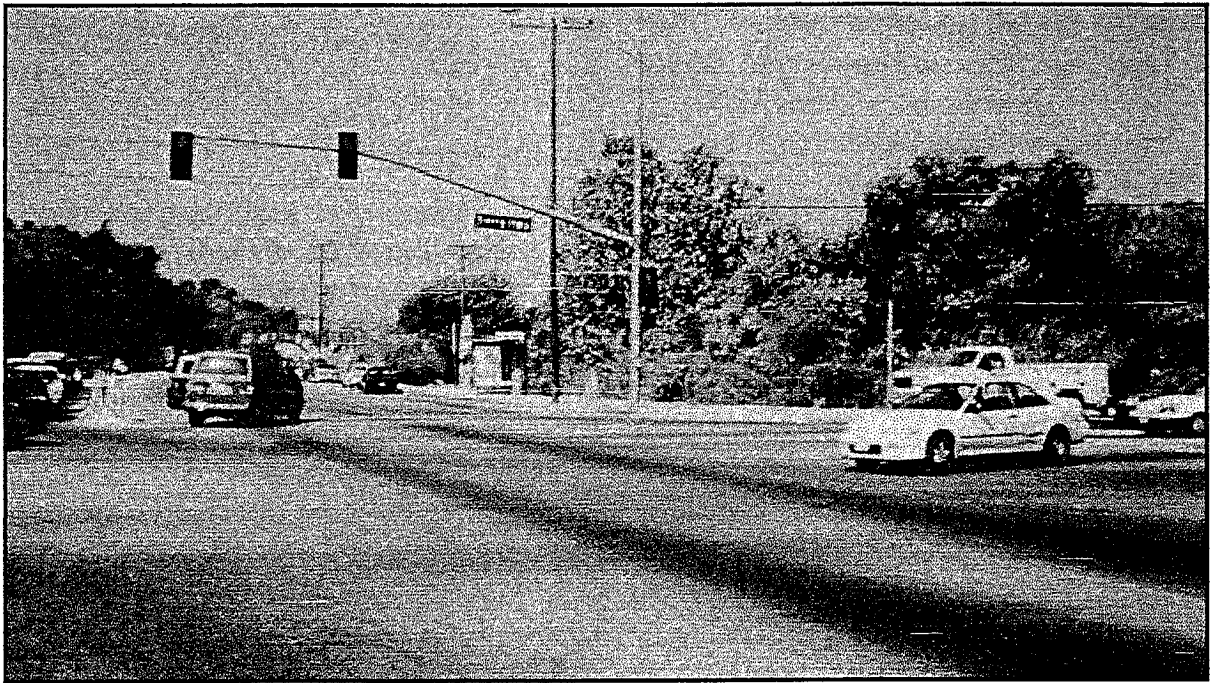
The "Bouquet Junction" and San Fernando Road / Sierra Highway intersections illustrated on the following pages depict the advantages of implementing some of these beautification concepts.



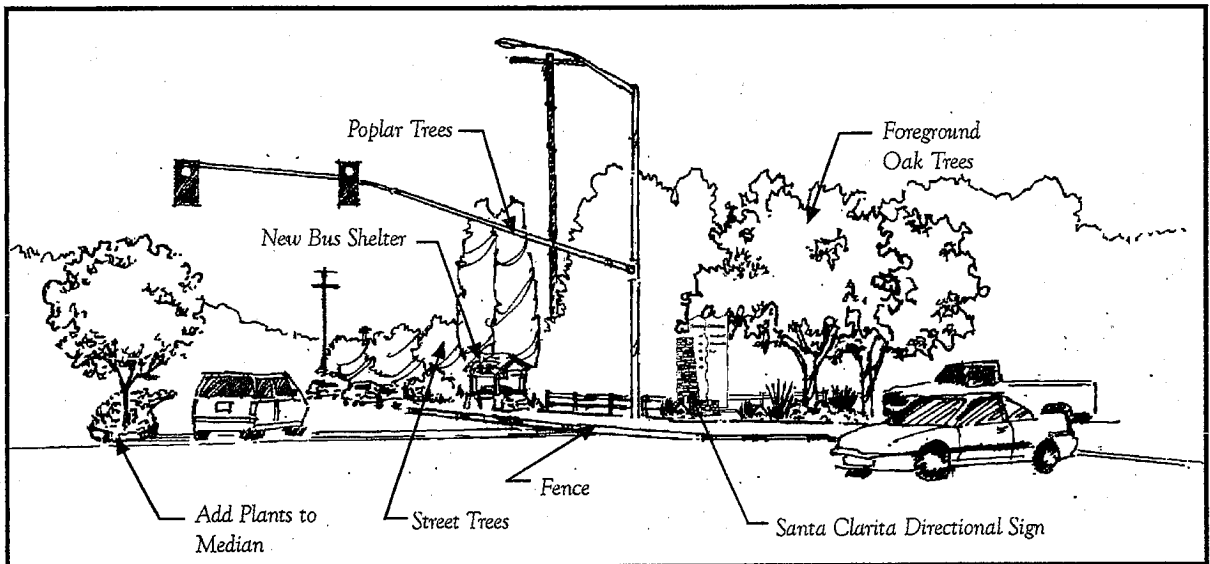
Before: "Bouquet Junction" Intersection



After: "Bouquet Junction" Intersection



Before: San Fernando Road and Sierra Highway Intersection



With implementation of beautification improvements: San Fernando Road and Sierra Highway Intersection Improvements with Directional Signage

5. Utility Lines

Overhead power and telephone lines extending across and along public rights-of-way interrupt views and detract from the overall aesthetic experience of corridors throughout the City of Santa Clarita. The City's Unified Development Code (UDC) requires that power lines and overhead cables less than 34 KV be installed underground (UDC - 17.15.020D and 17.15.040B). In addition, utilities are required to be placed underground for new development, whether on the urban fringe, or within an established area of the City. Utility undergrounding has also been conducted as part of the City's capital improvement program. Whenever possible, existing power and cable lines should be placed underground to reduce visual clutter.

6. Billboards / Advertisement Signs

Billboards, because of their sheer size and prominent location along major roadways, greatly detract from the natural environment and scenery experienced along public rights-of-way throughout Santa Clarita. For this reason, large billboards and advertising signs should be removed whenever possible. In the Community Design Element of the City's General Plan, several policies have been set in place to achieve Goal 8: To ensure that signage throughout the City is visually attractive and minimizes distraction. Specifically, Policy 8.4 calls for the prohibition of new billboards in the City and encourages the elimination of existing billboards citywide. In addition, Policy 8.2 directs the City to "work with appropriate state, federal and county agencies to prohibit billboards along freeway frontages and designated scenic highways." The Beautification Master Plan reinforces the policies set forth in the Santa Clarita General Plan and the City's commitment to billboard removal. In situations where development occurs on property containing billboards, the City should ensure that all billboards on site are removed as part of the project design.

7. Vacant Land

Blighted areas may consist of dilapidated vacant lots or open fields surrounded by development or located within an urban setting. It is recommended that the City use fencing (temporary) and landscaping to buffer vacant lots from the street scene. For example, three-rail fencing with vines and landscaping in the foreground would help create a consistent edge along major corridors, while screening vacant lots.

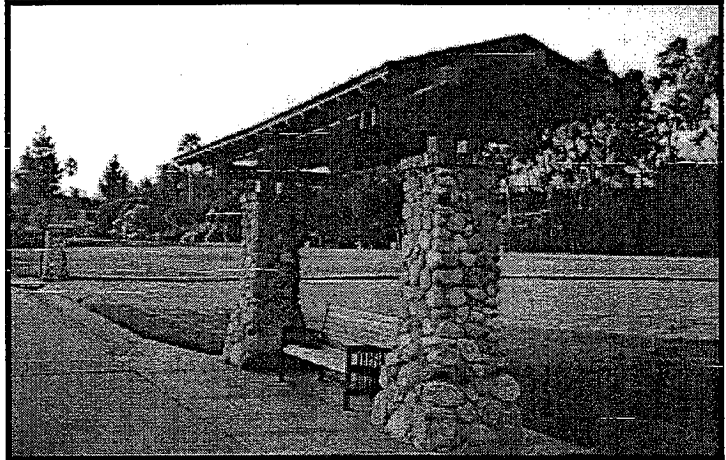
8. Fences / Walls Along City Rights-of-Way

There are numerous locations throughout Santa Clarita where homeowners have extended the height of their rear or side walls backing onto public rights-of-way. Along major corridors, these wall extensions have created a hodgepodge effect with a variety of fencing materials at a variety of heights. The City is encouraged to establish a uniform extension height, material and style and to provide homeowners with incentives to follow this established guideline. The new walls should be constructed of concrete block and should be a natural color. Texture variation in the block itself and the block layout is encouraged and a cap should be incorporated in the wall design. Uniform wall extensions would significantly improve the aesthetic environment for motorists traveling along Santa Clarita's vehicular corridors. Climbing vines are also strongly encouraged to be planted at the base of these rights-of-way at reasonable intervals. Vines would help to visually break-up large expanses of walls and provide added greenery to the corridor.

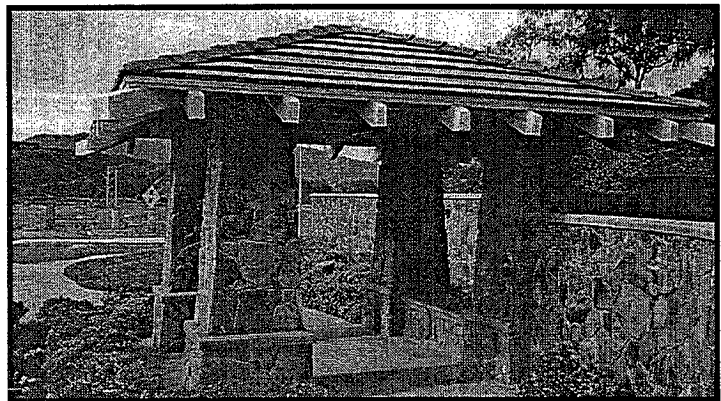
D. Public Transportation

1. Bus Stops, Benches and Shelters

Santa Clarita Transit (SCT) currently provides local transit service throughout the Santa Clarita Valley. The SCT provides service on every major road in Santa Clarita and there are over 600 bus stops in the local Santa Clarita transit system. Currently, bus benches, shelters, and trash cans are provided by outdoor advertising companies, who maintain the amenities in exchange for the revenue they collect from the advertising. As a result, most benches and shelters currently on the street are pre-fabricated, "off-the-rack" amenities, most of which feature advertising as a significant part of their design. The City has recently begun to require large development projects to construct permanent, stylized bus shelters, as well as bus benches and trash receptacles, at existing or planned bus stops adjacent to the proposed development. Such shelters reflect the architectural character of the development and help to achieve the goal of reducing the amount of advertising in the public right-of-way as well as obtain more attractive structures than the pre-fabricated units that are available.



Bus Shelter Example



Bus Shelter Example

Bus shelters, benches and trash receptacles are a very visible part of the City's streetscape and if they are implemented with a particular style or theme, they will become strong unifying elements for the City. Since they are found throughout the City and on every major corridor, it is critical that transit amenities are aesthetically pleasing and act as assets to the City. When the City initiates the replacement of existing structures, benches and trash receptacles or the placement of new transit features selected materials, colors and forms need to be unified. The following page describes the guidelines for future City-initiated bus stop/shelter design which will be provided and appropriately located as directed by the City. Bicycle racks will also be located at key bus stops and transit signs have been redesigned to incorporate route/schedules and bus stop signs. The City also has a transit logo which may be incorporated on the shelter.

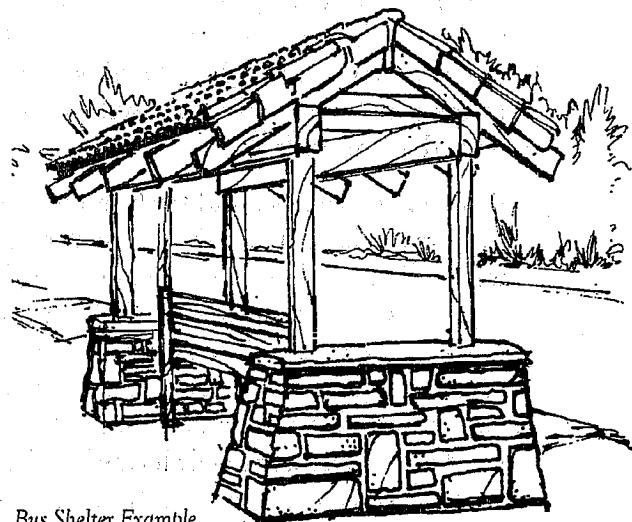
City-initiated bus shelters shall have the following characteristics:

- Shelters should incorporate a combination of urban and natural materials that reflect a blend of urban sophistication and rural ranch character. The materials should include: weathered metal; natural stone such as Bouquet Canyon stone or Vasquez Rock; heavy timber members; and tile or textured concrete.
- The Santa Clarita icon or transit logo should appear at each shelter as a plaque adhered to the bench or one placed prominently within the design of the shelter.
- A bench and trash receptacle should be placed at each bus stop. The bench materials should consist of wood slats with a preservative treatment and cast iron or concrete ends (or an alternate approved by the City of Santa Clarita).
- Benches should be secured to the ground surface with bolts and should include a wood-slatted back.
- Advertising is strongly discouraged on benches and trash receptacles and existing advertising benches should be replaced with non-advertising benches whenever possible.
- When “off-the-rack” shelters are selected that incorporate advertisements, tasteful applications of such features should be incorporated and the overall shelter must demonstrate the style that reflects the character of the City gateway.
- Custom shelters associated with new development may reflect the architectural style of the adjacent development, yet should still compliment the desired character of Santa Clarita. This character can be achieved through the use of materials and/or architectural form.

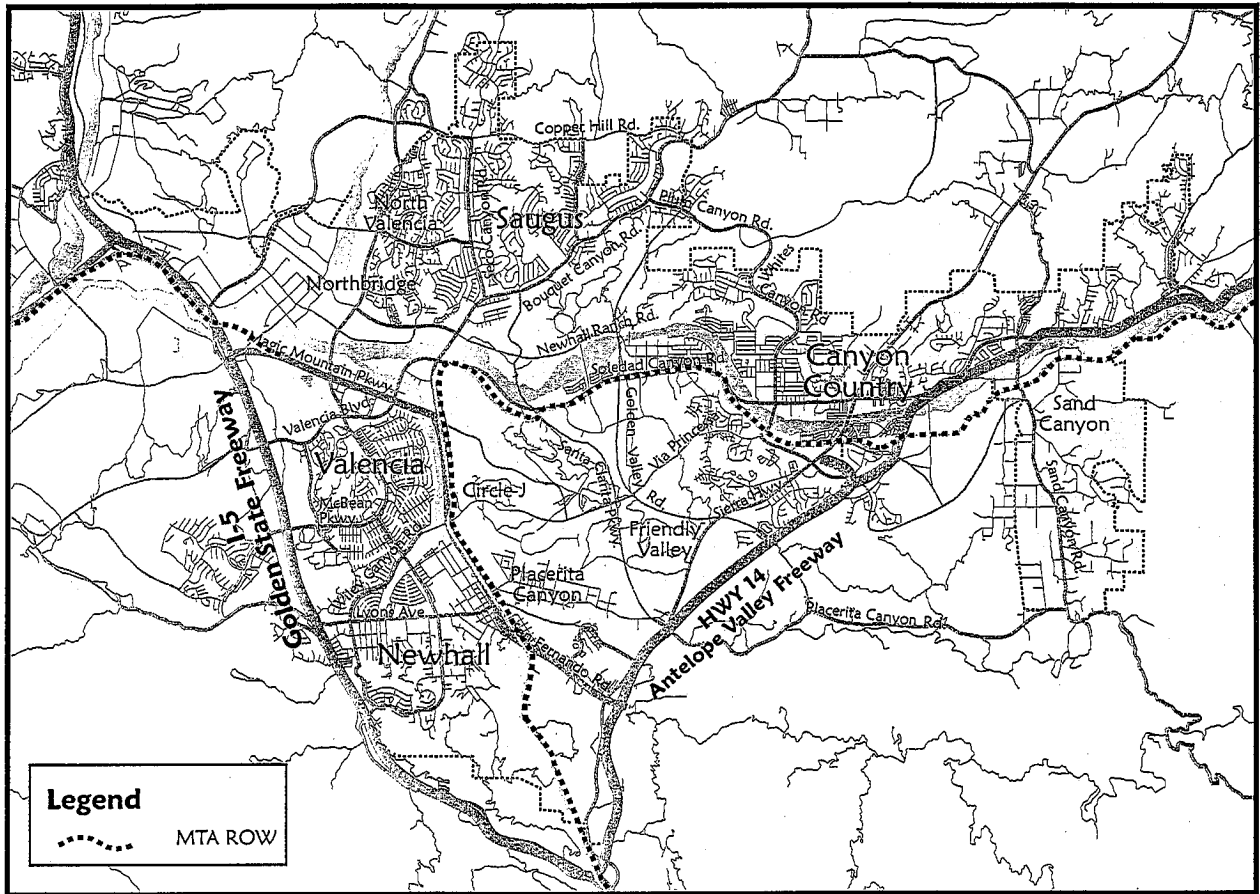


Santa Clarita Transit

Santa Clarita Transit Logo



Bus Shelter Example



MTA Right-of-way Map

2. Metropolitan Transportation Authority Right-of-Way

One of the goals of this Master Plan is to set forth long-range guidelines for the improvement, enhancement and beautification of Santa Clarita’s Metropolitan Transportation Authority (MTA) right-of-way corridors. The MTA right-of-way provides a common thread running both north/south and east/west in the City of Santa Clarita. If treated and enhanced properly the transportation corridor could provide visitors and residents alike with a clear sense of citywide identity, while linking individual communities.

Safety:

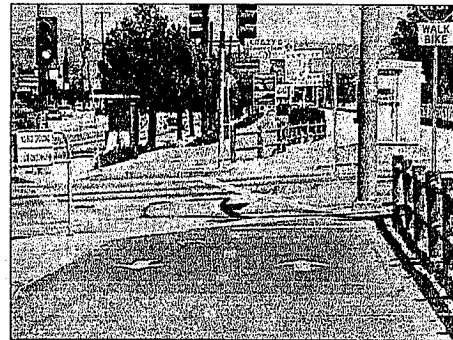
It is essential that any improvements to the right-of-way address pedestrian safety at intersections, railroad track crossings and along the tracks themselves. The use of accent paving at intersection crosswalks and aesthetically pleasing fencing along the corridor will help to provide a safe and unifying link throughout the City.

Fences:

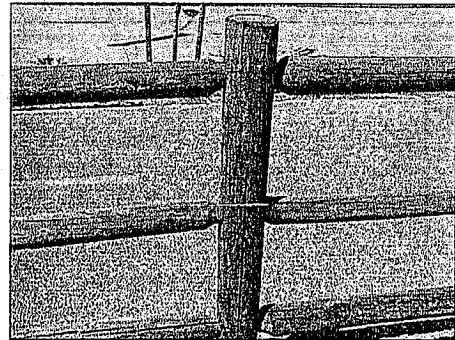
In addition to the fences used throughout Santa Clarita to unify the City, provide accent and help define a space, they can be used along river trails and to buffer railroad tracks. The same type of lodge pole fence that has been specified for Canyon Country has been used at some places along the MTA right-of-way. The continuation of this fence along the railroad tracks will provide a desired sense of continuity while creating a safer pedestrian environment. The following materials should be used for the lodge pole fence. Four inch diameter dowelled and treated pine or concrete should be used for rails and the diameter of the rail should be reduced at each end to match pre-drilled post holes.

Pedestrian and Bicycle System:

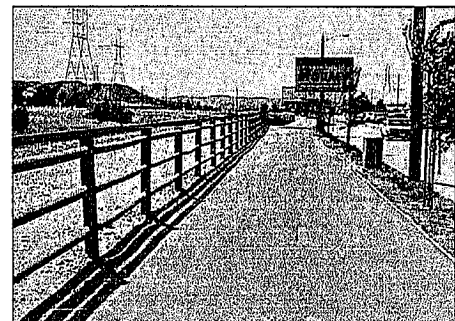
A pedestrian and bicycle system has been implemented along portions of the existing MTA right-of-way. Where space permits, a similar two-lane bicycle lane is encouraged to be created along the right-of-way. This system should be connected to other bicycle paths within the City.



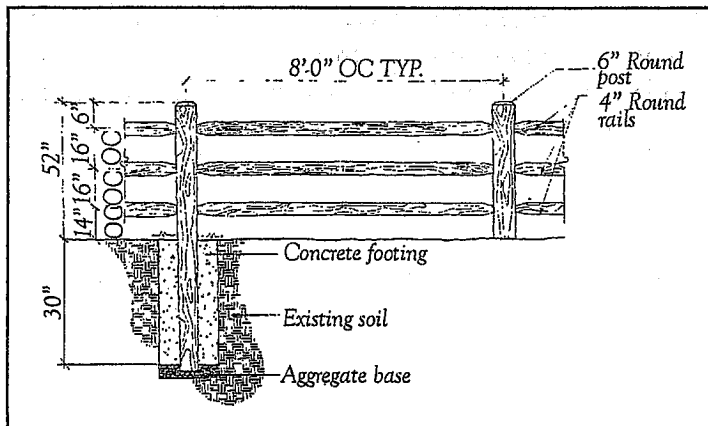
Example of Intersection to be Beautified and Made More Safe



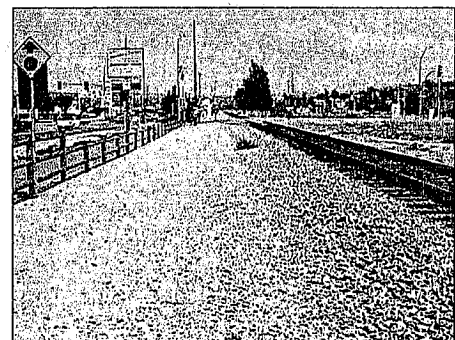
Example of Lodge Pole Fence



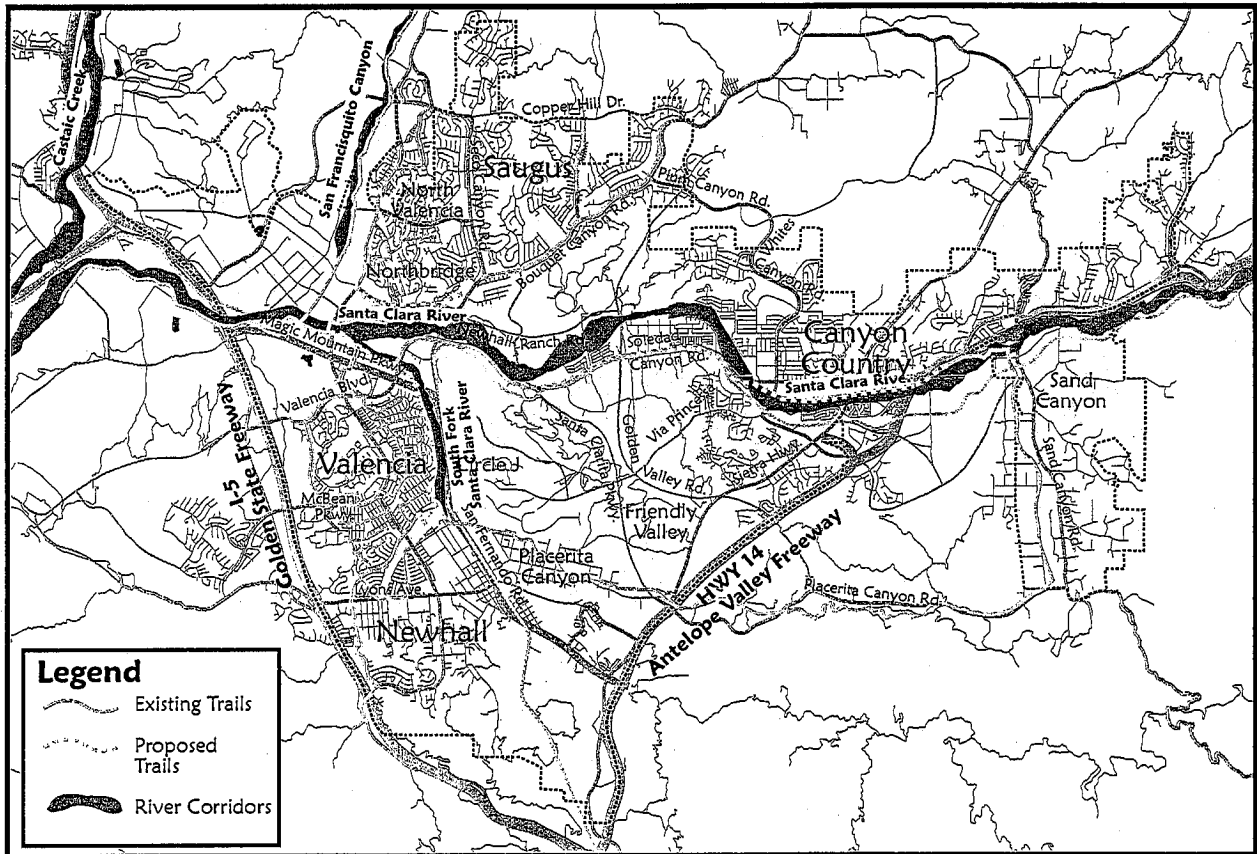
Example of Pedestrian and Bicycle System Adjacent to MTA Right-Of-Way



Lodge Pole Fence



Example of MTA Right-Of-Way



Regional Open Space Connectors Map

E. Open Space Connectors

The open space and recreational assets of the City of Santa Clarita are enhanced leisure areas that are visually attractive, rich in habitat and comfortable spaces to recreate. The Santa Clara River, including the south fork of the Santa Clara River and San Francisquito Creek extend through portions of the four individual communities and have the potential to act as unifying elements of the City. Enhancing these riparian corridors with the incorporation of key elements such as open space markers, fences, a specific tree palette for the trail system (refer to the *Citywide Landscaping Palettes*) and an accommodation for public art will link the existing system of equestrian, pedestrian and bicycle trails, as well as unify the City.

1. River, Bicycle, Pedestrian and Equestrian Trails

Preservation and enhancement of the Santa Clara River and open space areas are important to maintain the habitat value and charm that the natural environment brings to residents and visitors. The City has established a multi-use corridor system that includes a series of trails extending alongside the river. They serve a combination of users, including pedestrians, bicyclists and equestrians. It is the intent of these guidelines to strengthen the unifying potential of the existing trails.

- Similar to the gateway concept, trailheads should incorporate a group of elements that are repeated at each entry. These elements should include an open space marker (refer to the *Citywide Signage Program* section), a portion of a lodge pole fence, a trash can and

shrubs from the landscaping palette.

- Open space markers placed at trailheads could indicate the trail length, provide a trail map, provide a place to mount federal recreation symbols and incorporate the City icon.
- At every one-mile increment, another scene that incorporates an open space mile marker and a bench could be located. The markers will allow hikers, joggers, equestrians and bicyclists to know how far they have traveled and reinforce the connection to the City. Refer to the *Citywide Signage Program* section for additional information.
- Additional trees should be planted along the trails to strengthen the aesthetic appeal and sense of place established by the trail system. Refer to Trail System Landscaping in the *Citywide Landscaping Palettes* section of the document.
- Fences should be placed along the trails to provide unity along the corridors. They should be the same as those specified in the Metropolitan Transportation Authority right-of-way segment of the Public Transportation section of the *Citywide Design Guidelines*.

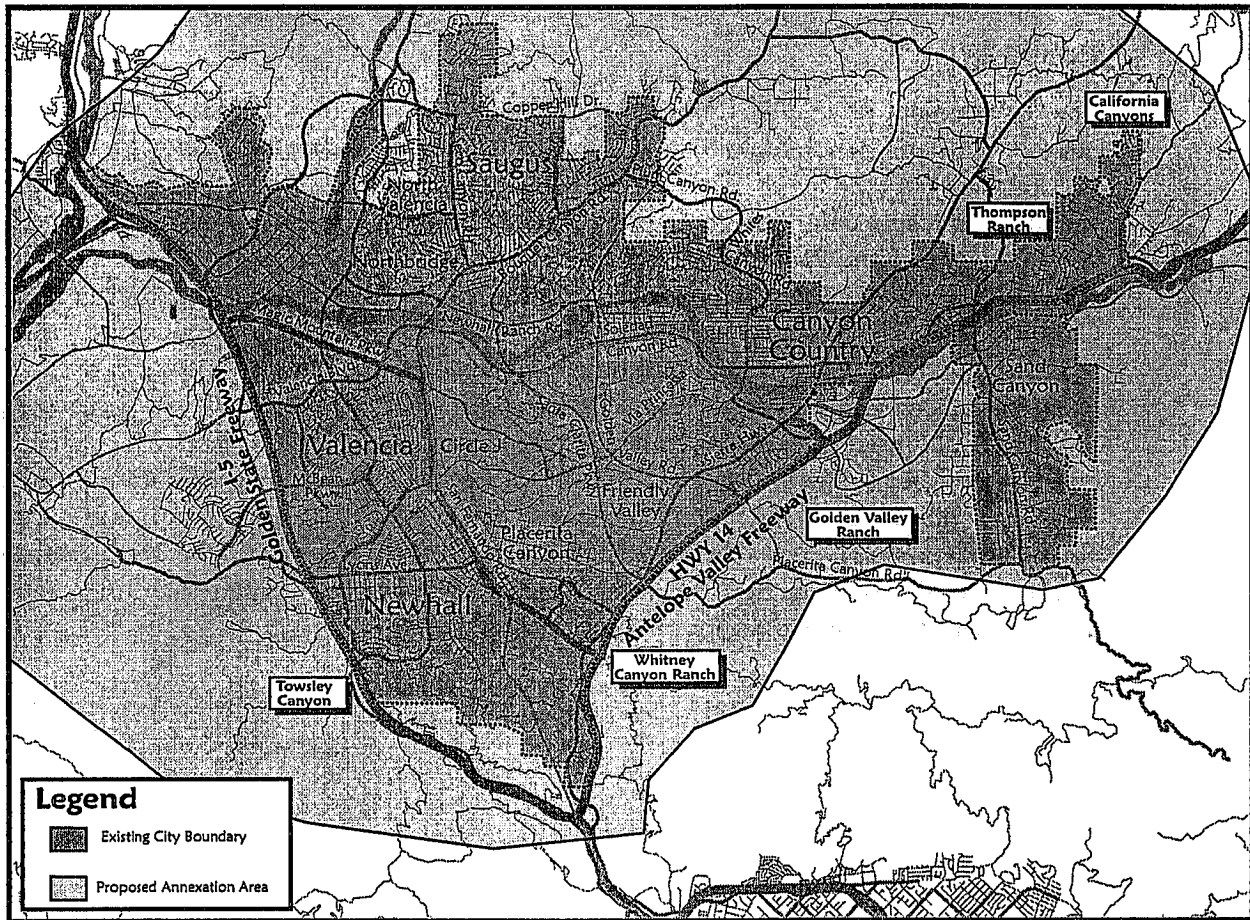
2. Bridges

Bridges provide a unique opportunity to experience and view landscapes and street scenes. Bridges are located where roads or pedestrian trails pass over corridors of activity. Since multi-use bridges can be found throughout Santa Clarita, they provide an excellent opportunity to unify the City. The implementation of monument markers or City icon plaques will bring together the four individual communities under the City umbrella.

- Railroad grade separations, major arterial interchanges and bridge structures over the Santa Clara River can provide large scale architectural elements of aesthetic interest, creating the interaction of straight and curved shapes which flow through space.
- Bridges (over crossings and under crossings) that are associated with one another should become a family of forms, but may vary slightly from one another to convey the impression that all were not built at the same time and that the community has evolved over time.
- The number of bridge support columns should be reduced wherever possible in order to create a lighter, more open appearance at the over and under crossings. It is important, however, that the thinness of the bridge structure be maintained.
- Barrier rails on bridge structures should appear to be integral with the structure of the road bed and should have a smooth, rounded edge which also decreases the apparent thickness of the structure edge.
- Bridge abutment walls should receive a pattern that is compatible with the wall and bridge treatments.
- Pilasters, light standards and fencing on the bridge structures are visually prominent features and should compliment the aesthetic of the structure.
- Slope paving occurs at over crossings, under crossings, alongside ramps and other areas adjacent to major road structures that are too narrow for landscaping. Surfaces of the slope paving should be treated to compliment the bridge abutment walls.

3. Public Art

The development of an “Art In Public Places” program requiring the design of future public improvement and private development projects to incorporate a public art component could be established in the City of Santa Clarita. When implemented, the City’s public art program would significantly contribute to the arts and cultural resources of the City of Santa Clarita and complement the Beautification Master Plan. Public art can be geared toward “cultural representations” of the area, which focus on the historic and indigenous character of the Santa Clarita Valley. This presents an opportunity not only to facilitate desirable public spaces and aesthetically improve streetscapes and other public corridors, but also to celebrate the City’s historic character and cultural diversity. By recognizing key historic individuals and events, this program could insert the area’s cultural heritage into the everyday life of the City. Whether in a public park, along the trail system, or in an office plaza, public art contributes to providing a sense of place. The trail system in Santa Clarita should accommodate public art with spaces made available at trailheads and at key locations along the trail. The repetition of public art would enhance the City’s open space corridors, provide interest to users and offer another element to unify the City.



Proposed Annexation Areas Map

F. Proposed Annexation Areas

In an effort to further unify the City and create a framework for future development, the *Citywide Design Guidelines* should be applied to all future annexation areas. In addition, developments and streetscape improvements taking place in the areas depicted in the exhibit above should refer to the corresponding *Community Design Guidelines*. This will ensure that as communities grow, the established sense of continuity is retained. Any future annexations not indicated in the exhibit should follow the guidelines of the community located in closest proximity.

G. Citywide Landscaping Palettes

Landscaping provides a buffer between uses as well as beautification along roadways. All landscaping along City rights-of-way shall be installed and maintained in accordance with City Ordinance 90-15. All landscaping is subject to review and approval by the Director of Parks, Recreation and Community Services.

The following landscaping palette has been created using and enhancing existing plant palettes applied throughout the Santa Clarita Valley. Future planting should utilize this list or plants that are similar and compliment the desired theme.

1. Gateway Landscaping

Trees

Botanical Name	Common Name
<i>Populus fremontii</i>	Western Cottonwood
<i>Quercus lobata</i>	Valley Oak

Shrubs

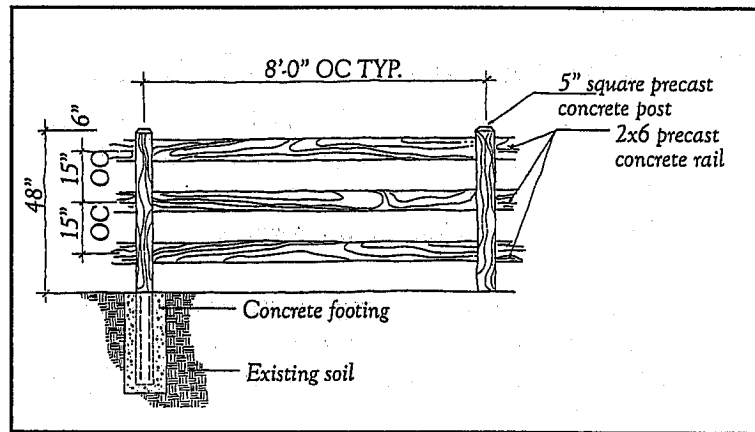
Botanical Name	Common Name
<i>Nerium oleander</i> 'Petite Pink'	'Petite Pink' Oleander
<i>Pennisetum setaceum</i>	Fountain Grass
<i>Phormium tenax</i> 'Maori Queen' Dwarf	New Zealand Flax
<i>Rosmarinus officinalis</i>	Rosemary
<i>Tagetes lemmonii</i>	Bush Marigold
<i>Teucrium fruticans</i> 'Compactum'	Bush Germander
<i>Pittosporum tobira</i> 'Wheeler's Dwarf'	Tobira
<i>Plumbago auriculata</i>	Cape Plumbago
<i>Pyracantha</i> 'Santa Cruz'	Firethorn
<i>Raphiolepis indica</i> 'Springtime'	India Hawthorn
<i>Xylosma congestum</i> 'Compacta'	Shiny Xylosma

Ground Cover

Botanical Name	Common Name
<i>Arctostaphylos</i> 'Emerald Carpet'	Dwarf Manzanita
<i>Campanula</i> sp.	Bellflower
<i>Cotoneaster dammeri</i> 'Lowfast'	Bearberry Cotoneaster
<i>Festuca ovina glauca</i>	Blue Fescue
<i>Gazania</i> 'Mitsuwa'	Trailing Gazania
<i>Hedera helix</i> 'Hahn's Self Branching'	Hahn's Ivy
<i>Hedera helix</i> 'Needlepoint'	Needlepoint Ivy
<i>Helianthemum nummularium</i>	Sun Rose
<i>Myoporum parvifolium</i> 'Putah Creek'	---
<i>Rosmarinus officinalis</i> 'Prostratus'	Rosemary
<i>Vinca major</i>	Periwinkle
<i>Zoysia tenuifolia</i>	Korean Grass

2. Fences

Precast concrete 2x6 rails should be used for horizontal members set into predrilled 5" square posts placed 8' on center and set in a concrete footing for maximum stability. All fencing members should be steel reinforced, integrally colored and have a rugged wood grain texture on all sides.



Three-rail Fence Detail

3. Parkway Landscaping
Trees

Magic Mountain Parkway

Botanical Name

Lagerstroemia indica

Schinus molle

Common Name

Crape Myrtle

California Pepper

Valencia Boulevard

Botanical Name

Eucalyptus sideroxylon

Pistacia chinensis

Common Name

Red Ironbark

Chinese Pistache

Lyons Avenue

Botanical Name

Fraxinus velutina

Pyrus calleryana 'Bradford'

Common Name

Arizona Ash

Bradford Pear

Bouquet Canyon Road

Botanical Name

Geijera parviflora

Prunus cerasifera 'Atropurpurea'

Quercus lobata

Robinia pseudoacacia

Common Name

Australian Willow

Purple Leaf Plum

Valley Oak

Black Locust

Soledad Canyon Road

Botanical Name

Liquidambar styraciflua

Prunus cerasifera 'Atropurpurea'

Quercus ilex

Robinia pseudoacacia

Common Name

Sweet Gum (Canyon Country)

Purple Leaf Plum

Holly Oak (Center City)

Black Locust

Parkway Landscaping, continued . . .

San Fernando Road

Botanical Name

*Geijera parviflora**Pyrus kawakamii**Pyrus calleryana* 'Bradford'

Common Name

Australian Willow

Evergreen Pear

Bradford Pear

Shrubs

Botanical Name

*Arctostaphylos species**Abelia grandiflora**Buxus microphylla japonica**Cistus species**Convolvulus cneorum**Dietes vegeta**Dodonaea viscosa**Grevillea rosmarinifolia**Hemerocallis hybrids**Heteromeles arbutifolia**Juniperus species**Leptosermum laevigatum**Ligustrum j. texanum**Limonium perezii**Mahonia pinnata* 'Ken Hartman'*Nerium oleander species**Osmanthus fragrans**Pennisetum setaceum**Phormium tenax species**Photinia fraseri**Plumbago auriculata**Prunus lyonii**Raphiolepis species**Rhamnus californica**Rosmarinus species**Stipa species**Tagetes lemmonii**Teucrium fruticans**Westringia fruticosa**Xylosma congestum*

Common Name

Manzanita

Glossy Abelia

Japanese Boxwood

Rockrose

Bush Morning Glory

Fortnight Lily

Hop Bush

Rosemary Grevillea

Day Lily

Toyon

Junipers

Australian Tea Tree

Japanese Privet

Statice

California Holly Grape

Oleander

Sweet Olive

Fountain Grass

New Zealand Flax

Photinia

Cape Plumbago

Catalina Cherry

India Hawthorn

Coffeeberry

Rosemary

Feather Grass

Bush Marigold

Bush Germander

Westringia

Shiny Xylosma

Parkway Landscaping, continued . . .

Ground Covers

Botanical Name	Common Name
<i>Achillea</i>	Yarrow
<i>Acacia redolens</i>	Acacia
<i>Arctostaphylos species</i>	Manzanita
<i>Cotoneaster species</i>	Cotoneaster
<i>Fragaria chiloensis</i>	Ornamental Strawberry
<i>Gazania species</i>	Gazania
<i>Hedera helix</i>	English Ivy
<i>Helianthemum nummularium</i>	Sun Rose
<i>Juniperus species</i>	Juniper
<i>Lantana montevidensis</i>	Latana
<i>Myoporum 'Pacificum'</i>	Myoporum
<i>Myoporum parvifolium</i>	Myoporum
<i>Rosmarinus officinalis prostratus</i>	Rosemary
<i>Trachelospermum jasminoides</i>	Star Jasmine
<i>Zoysia tenuifolia</i>	Korean Grass

Vines

Botanical Name	Common Name
<i>Ficus pumila</i>	Creeping Fig
<i>Macfadyena unguis-cati</i>	Cats Claw Vine
<i>Parthenocissus tricuspidata</i>	Boston Ivy
<i>Solanum jasminoides</i>	Potato Vine

4. Trail System Landscaping

Trees

Botanical Name	Common Name
<i>Alnus rhombifolia</i>	White Alder
<i>Cedrus deodara</i>	Deodar Cedar
<i>Geijera parviflora</i>	Australian Willow
<i>Koelreuteria bipinnata</i>	Chinese Flame Tree
<i>Koelreuteria bipinnata</i>	Chinese Flame Tree
<i>Pinus brutia</i>	Calabrian Pine
<i>Pinus canariensis</i>	Canary Island Pine
<i>Platanus racemosa</i>	California Sycamore
<i>Populus fremontii</i>	Western Cottonwood
<i>Prunus cerasifera 'Atropurpurea'</i>	Purple Leaf Plum
<i>Quercus agrifolia</i>	Coast Live Oak

Trail System Landscaping, continued . . .

Trees (cont.)

Botanical Name

Quercus douglasii
Umbellularia californica
Pinus eldarica

Common Name

Blue Oak
California Laurel
Afghan Pine

Shrubs

Botanical Name

Cistus species
Oleander 'Petite Salmon'

Common Name

Rockrose
Petite Salmon Oleander

CANYON COUNTRY

Canyon Country

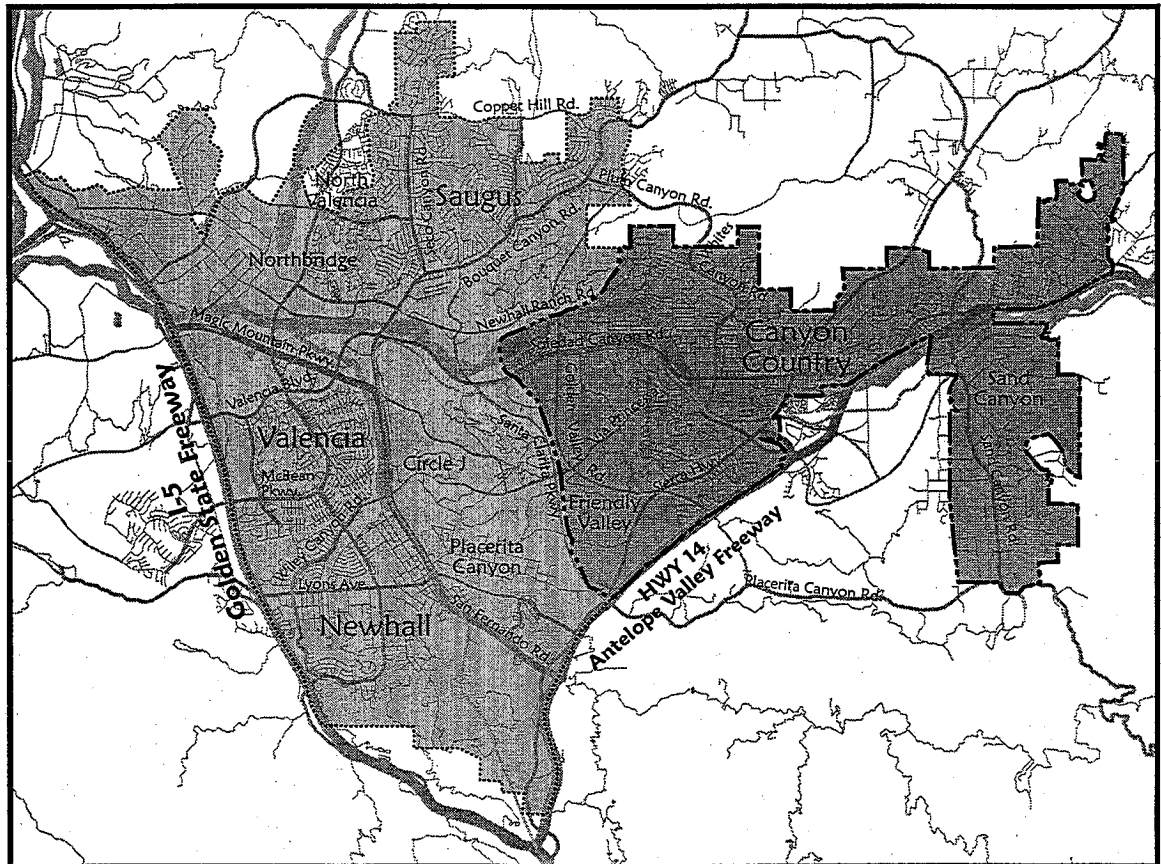
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II. Canyon Country Community Design Guidelines

A. Introduction

The following beautification guidelines contain specific recommendations for the community of Canyon Country. Where there is overlap with the *Citywide Design Guidelines*, cross references are made. Though there is not a clear boundary for Canyon Country, the map below delineates the general area. These guidelines focus on areas within the public right-of-way in Canyon Country.



Community of Canyon Country Boundary

1. Community Identity

The community of Canyon Country deems itself “The Best Kept Secret of the Valley.” It is a place full of community pride and rural character. Surrounded by a series of mountains and canyons that penetrate the community, the location contributes to community members’ appreciation for the natural environment. In addition to natural sensitivity, Canyon Country is a community that encourages economic and cultural diversity and is perceived by locals as a rural community with an essence of sophistication.

The following characteristics were used to describe Canyon Country during a public workshop.

- Family-oriented
- Rural
- Rustic
- Equestrian
- Country
- Mountainous
- River trails
- Culturally diverse
- Economically diverse
- Estate homes
- Large lots
- Rock precipice
- Ranch / rugged
- Outdoor-oriented
- Natural
- Best kept secret
- Small town feel
- Community involvement
- Friendly

2. Beautification Needs

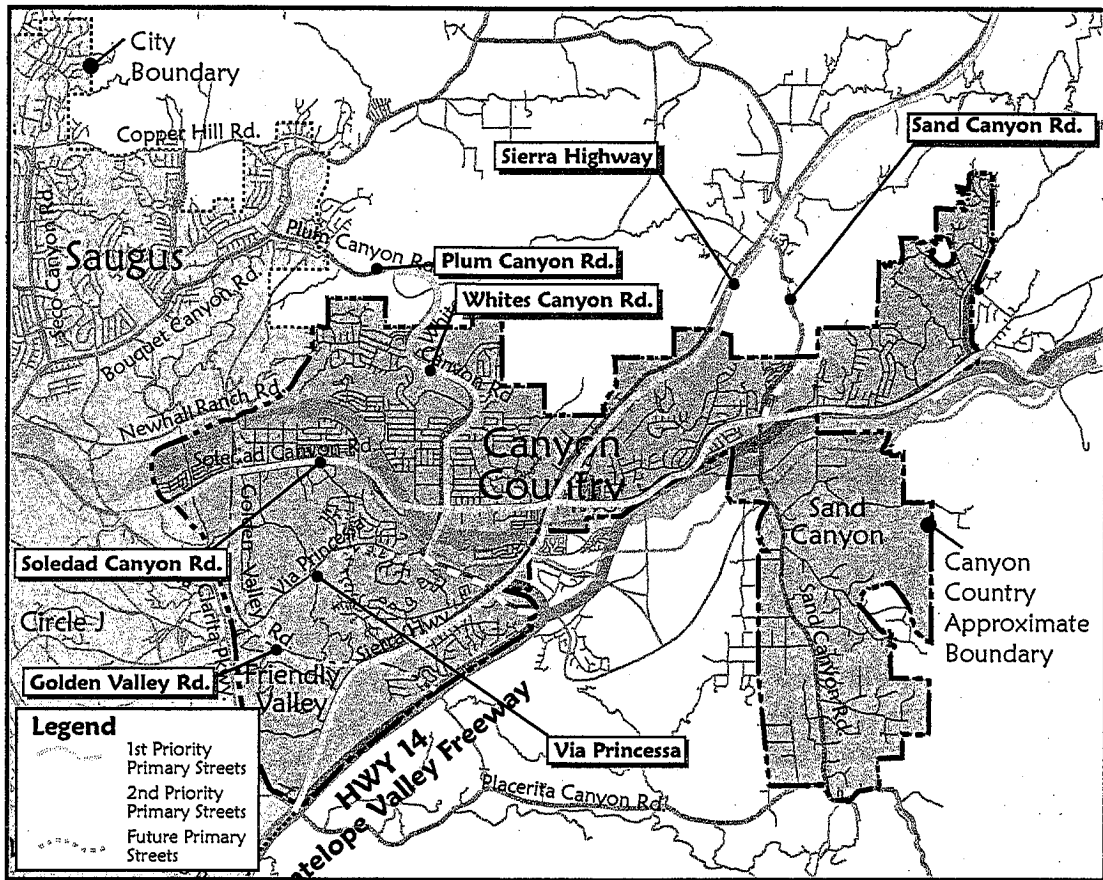
Recognizing the need for beautification in their community, Canyon Country has organized a number of volunteer groups that have committed many hours of hard work to help define community beautification goals and identify specific needs. These groups have contributed significantly to the contents of these guidelines.

The following list of beautification needs was generated at a public workshop conducted with members of the community, members of the Canyon Country volunteer organizations and Santa Clarita planning staff.

- Vacant lots should be screened.
- Caltrans / Los Angeles County areas should be beautified.
- Bus stops should be beautified and trash cans should be added.
- Key intersections should be designed consistently with crosswalks, landscaping, amenities and lighting.
- Blank walls should be screened with landscaping, including vines.
- Street parkways and medians should be planted and median treatments should be extended to the edges of the community.
- The Santa Clara River should be exposed and taken advantage of as an amenity.
- The amount of visual clutter including signs, overhead utilities and vehicular access points should be reduced.
- The strip commercial look of Soledad Canyon Road should be improved.
- A logo that will help to unify Canyon Country should be developed.
- Community entry signage should be designed with a logo image, colors and font that represents Canyon Country.
- Community identity should be strengthened, but not at the expense of citywide unification.

Based on this community feedback, the following beautification guidelines were drafted to address the following topics. The guidelines address a range of design improvements that can lead to a more cohesive and unified look for Canyon Country.

- | | | |
|----------------------|---------------------------|-------------------------------|
| 1) Primary corridors | 4) Street edge treatments | 7) Main intersections |
| 2) Signs | 5) Fences | 8) Other beautification ideas |
| 3) Medians | 6) Community entries | |



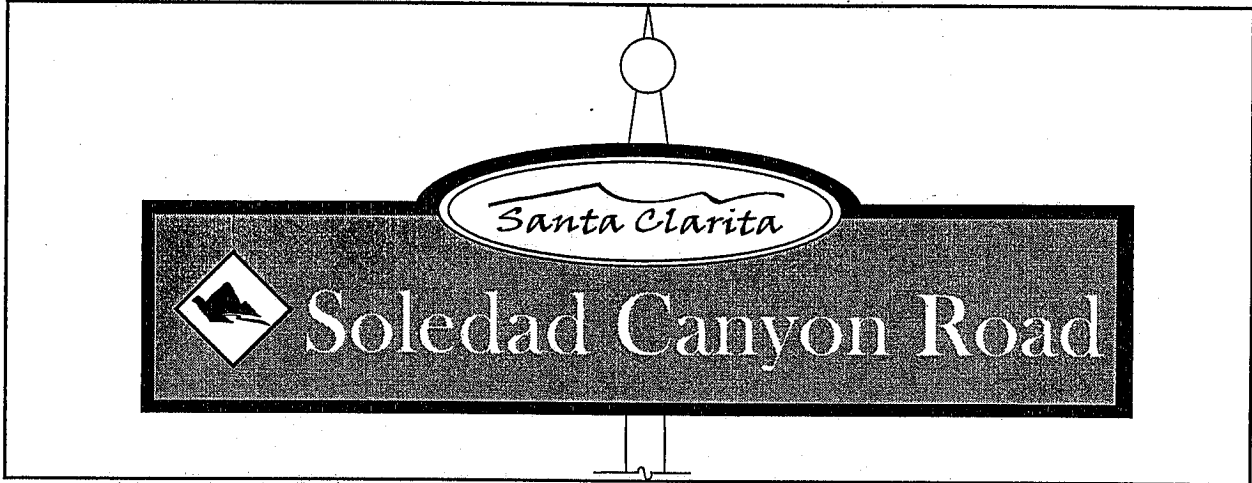
Canyon Country Primary Streets

B. Treatment of Primary Corridors

The primary corridors that extend through a given community act as the most visible and consequently the strongest unifying element within the community. Each community within the City of Santa Clarita should be clearly identifiable with distinct primary corridors that help unify it. The implementation of a community logo on each street sign, a uniform median and corridor edge treatment, a unique fence type and landscaping improvements bring together the image of a cohesive community. One of the goals of the Beautification Master Plan is to set forth long-range guidelines for improvements, enhancements and beautification of such corridors so that implementation occurs in a direction that provides a clear and easily recognizable individual community. The following streets have been designated as primary corridors within Canyon Country:

- First Priority Primary Streets: Whites Canyon Road / Plum Canyon Road, Soledad Canyon Road, and Sierra Highway
- Second Priority Primary Streets: Via Princessa, Sand Canyon Road, and Placerita Canyon Road
- Future Primary Streets: Golden Valley Road, and Via Princessa

1. Signs and Logo



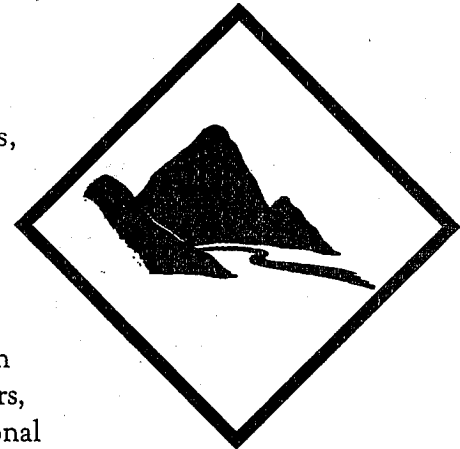
Example of a Street Sign Designed to Incorporate a Canyon Country Logo

Street Sign:

The most prevalent and obvious component of the primary corridor elements is the street sign treatment. It is here that a clear and distinct demarcation occurs for each individual community. Every street sign within the community of Canyon Country should contain a community logo. This logo provides a recognizable sense of repetition that clearly enables a motorist, bicyclist or pedestrian to know where they are and if they are still within the community limits. The logo will be placed within a diamond on the left side of the sign and each diamond will be a specific color for each community. This simple and quickly recognizable feature will provide a highly visible statement as to the community that an individual is in. Refer to the *Citywide Signage Program* for further information.

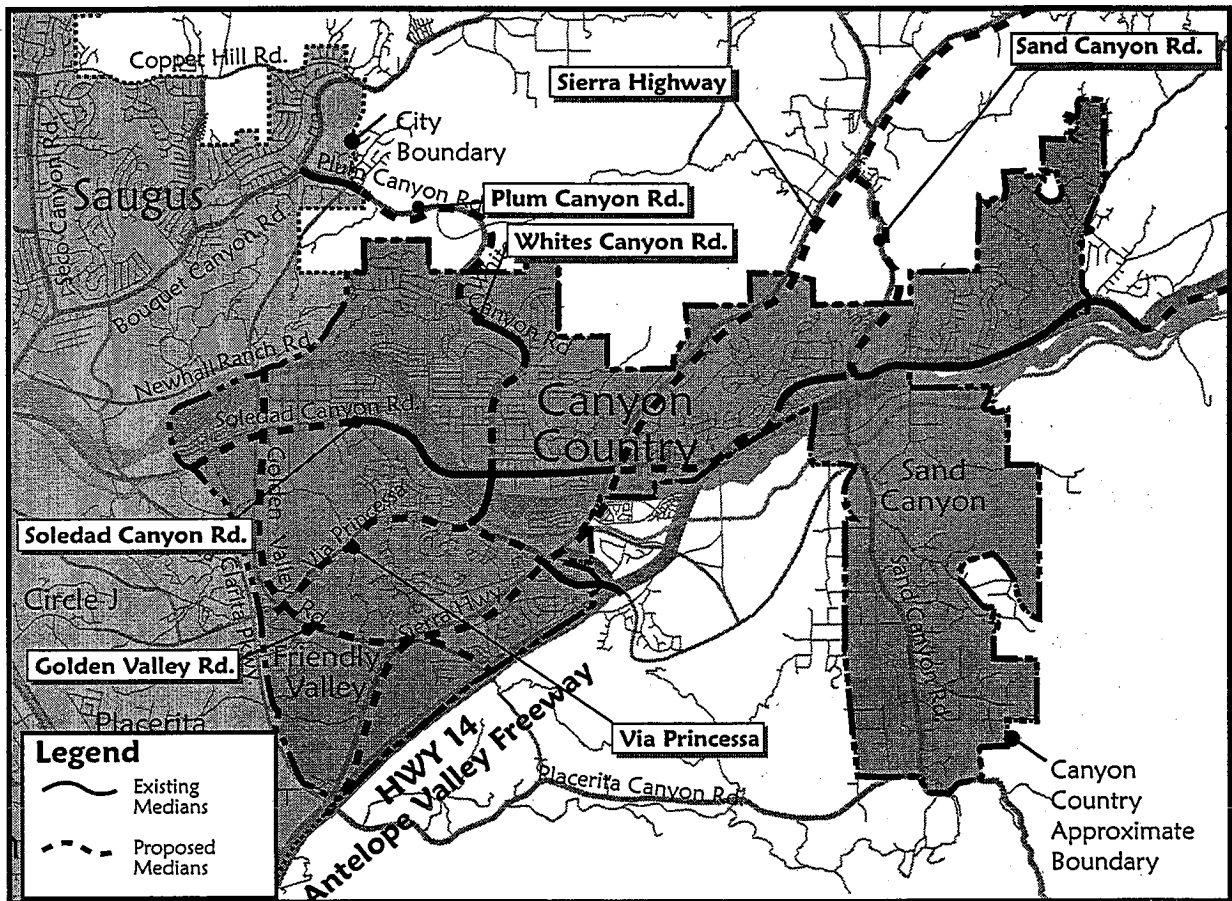
Canyon Country Community Logo:

Through the interview and public outreach process, participants generated a list of ideas for a Canyon Country logo. Of the proposed logos listed below, the majority of the participants agreed that an icon that incorporates a river, mountains, canyons, or oak trees would be most appropriate and representative of the community. The logo should incorporate an icon or symbol that is timeless, avoiding trendy images, colors, or fonts. A specific logo will be designed with additional community input at the time of implementation, however, some possibilities include:



Example of Possible Canyon Country Logo

- Oak tree
- Mountains
- Canyon Country brand
- Sunshine
- 20-mule team borax
- Honey bee industry
- River
- Railroad history
- Vasquez Rocks
- Canyons



Canyon Country Medians Map

2. Median Treatment

One intent of the Beautification Master Plan is to build on and extend Santa Clarita’s existing network of roadway medians into one, cohesive citywide plan that identifies each community. Each community will have a planting theme for all medians to promote an identity within the area. The plant palette selection that will be used in the beautification and implementation of the medians will continue to enhance the adjacent landscape. The materials shall be consistent within a given community and act as a clear indicator of the difference between communities. New raised and planted medians should be considered a long-term objective for community beautification. Along with providing aesthetic appeal and consistency, the proposed medians will also control vehicle circulation, movements and serve to calm traffic. The medians will also provide a place for roadway approach street signs and directional signage at key intersections as part of the citywide signage program discussed in the *Citywide Design Guidelines*. Within Canyon Country and vicinity, the selected median treatment will be applied to the following roads at the locations indicated on the figure above.

- Via Princessa - Golden Valley Road east to existing median
- Whites Canyon Road / Plum Canyon Road - fill gaps between existing medians
- Soledad Canyon Road - fill gaps between existing medians
- Golden Valley Road - Newhall Ranch Road south to Highway 14
- Sierra Highway - Santa Clarita Parkway to the north

Currently, median maintenance is funded by either the City General Fund or privately-funded Landscape Maintenance Districts (LMD). These different funding sources provide opportunities for different median design concepts. Both designs incorporate a similar plant palette that will help to enhance and unify the community. A drought tolerant design offers a lower maintenance solution with plants that have an ability to thrive in tough conditions and require less water. A wide variety of plants have been selected that can add beauty and diversity to any landscape. A combination of crescent shaped landscaped pockets and stamped concrete paving should continue to be used in areas that are funded by the City General Fund. The LMD medians will use a similar plant palette, but with less stamped concrete.

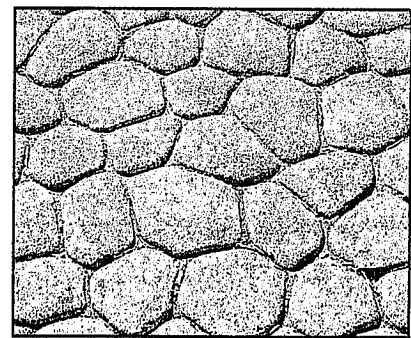
The median treatment that has been selected for Canyon Country is a drought tolerant landscaping design that incorporates a drip irrigation system and natural boulders. The landscaping is planted in crescent shaped pockets and is surrounded with a stamped concrete safety curb that is shaped like river rock and laid in a four-to-six inch pattern. The concrete is natural grey and speckled with obsidian and burnt orange lithochrome color hardener.



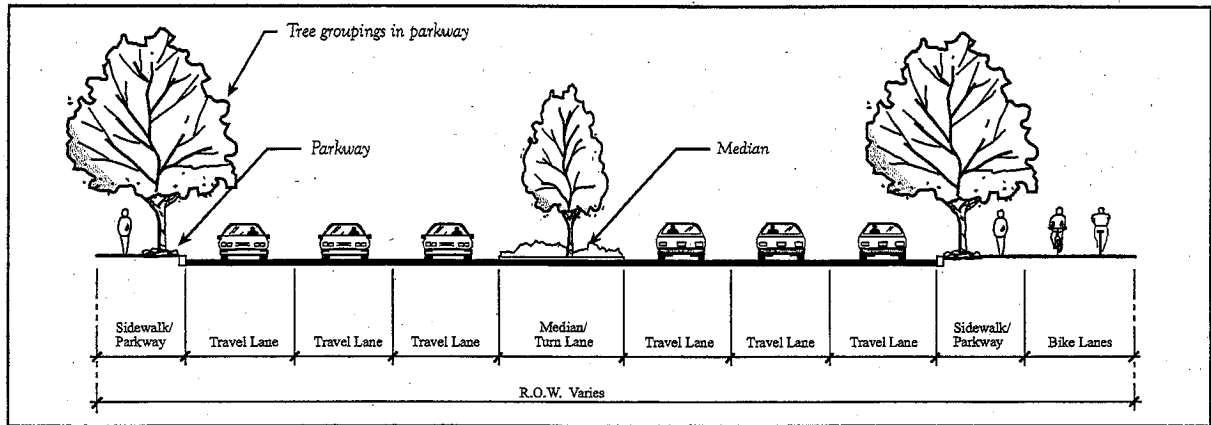
Median Treatment with Drought Tolerant Landscaping



Example of Canyon Country Median Treatment



River Rock Safety Strip



Example of Roadway Section

The following landscaping palette has been created using and enhancing existing plant palettes used within the area. Future planting should utilize this list or plants that are similar and compliment the desired theme.

Median Landscaping

Trees types that are being used

Botanical Name

Prunus cerasifera 'Atropurpurea'

Quercus agrifolia

Robinia ambigua Idahoensis

Common Name

Purple Leaf Plum

Coast Live Oak

Idaho Locust

Shrubs which are being used

Botanical Name

Dietes vegeta

Pittosporum tobira 'Wheeler's Dwarf'

Raphiolepis indica

Rosmarinus officinalis

Myoporum parvifolium 'Putah Creek'

Common Name

Fortnight Lily

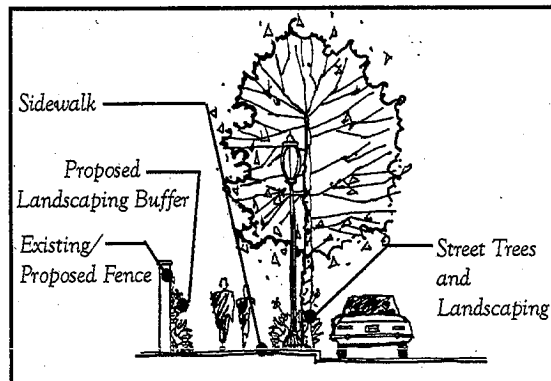
Dwarf Tobira

India Hawthorn

Rosemary

3. Street Edge Treatment

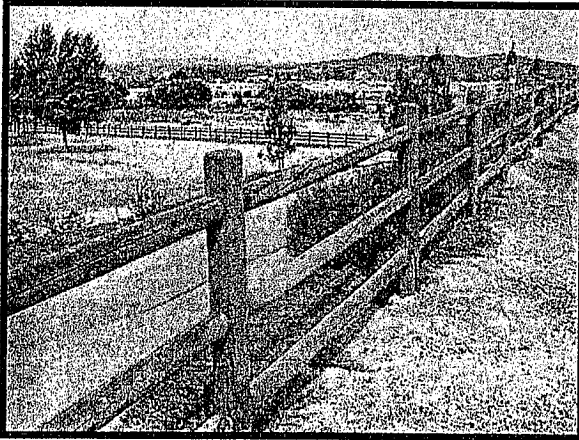
The sides of the primary corridors are just as important as the median treatment. Well landscaped parkways provide pleasant driving and walking environments while creating a sense of place. All planting shall be consistent with City Ordinance 90-15 and shall be reviewed and approved by the Director of Parks, Recreation and Community Services. A planting palette for such parkways is included in the Citywide Landscaping Palettes section of the document.



Example of Street Edge Treatment

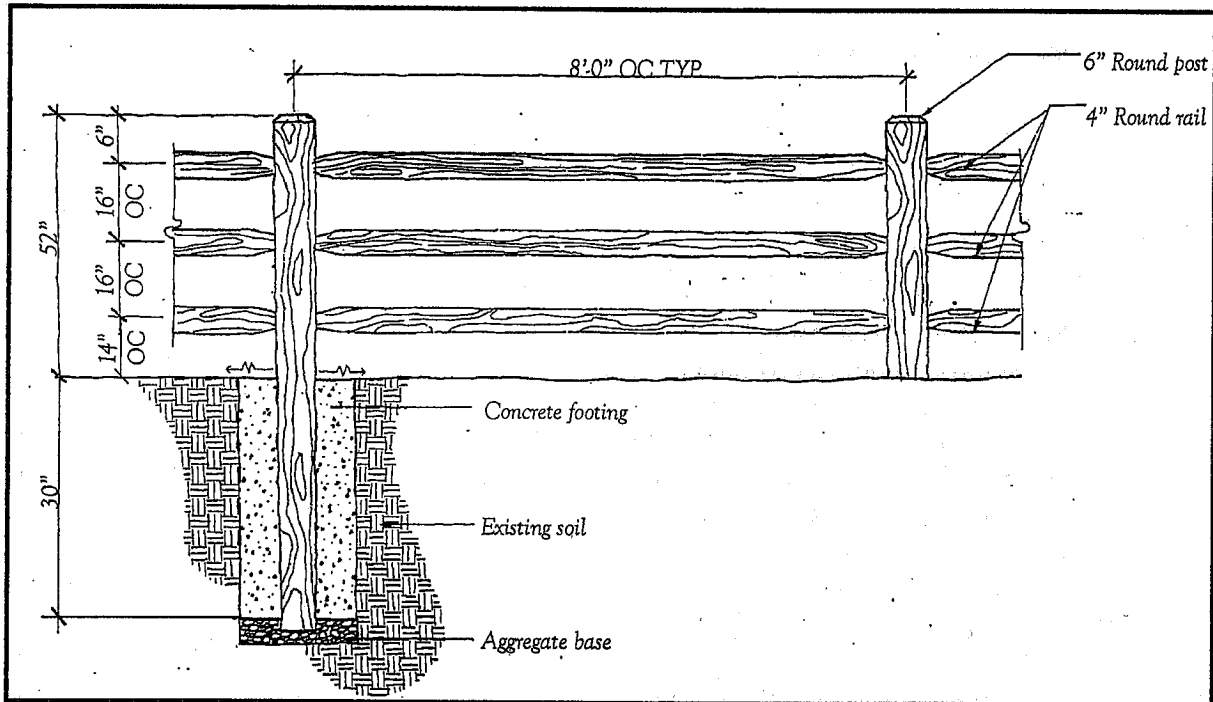
Fences:

Fences should be used to unify the City of Santa Clarita. They provide accent and help define a space and can be used to buffer parking lots adjacent to the roadway. In addition, fences used at vacant lots can “bridge” large void spaces and provide unity along the street scene. Each community should incorporate its own fencing type at intersections and along primary corridors where appropriate.

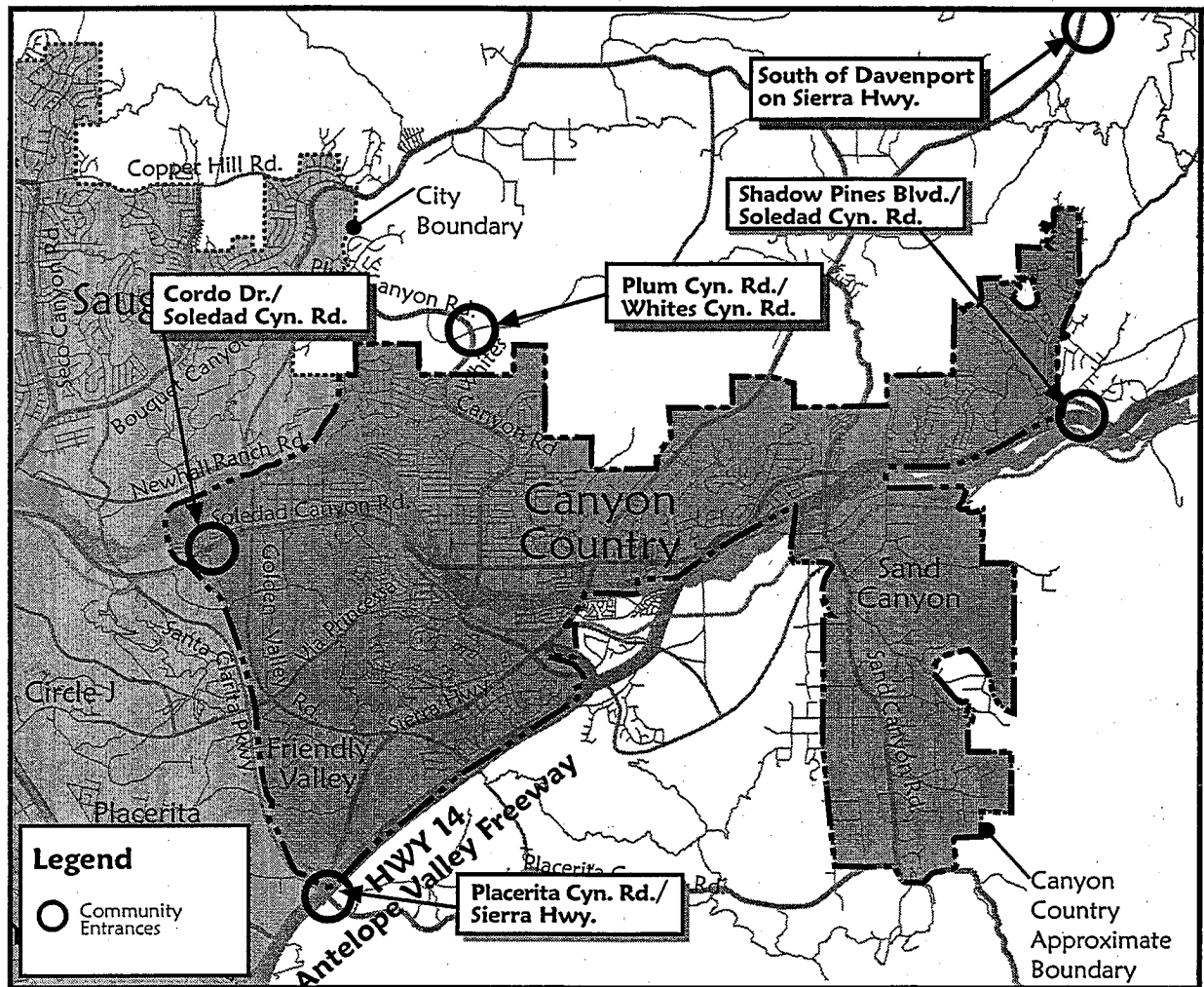


The natural lodge pole fence selected for Canyon Country blends nicely with the landscaping and reflects the rural character of the community. The following materials should be used for the lodge pole fence. Four inch diameter dowelled and treated pine or concrete should be used for rails and the diameter of the rail should be reduced at each end to match pre-drilled post holes.

Natural Lodge Pole Fence Example



Lodge Pole Fence Detail



Canyon Country Entrances Map

C. Community Entrances and Intersections

1. Community Entrance Treatment and Locations

The entrance to Canyon Country should have a unique and easily identifiable scene that includes specific landscaping, fence /wall and a unique community-oriented sign. The entrances should give visitors and residents a feeling of what to expect within the community and be welcoming without being overstated. The following locations have been selected for community entrance treatments.

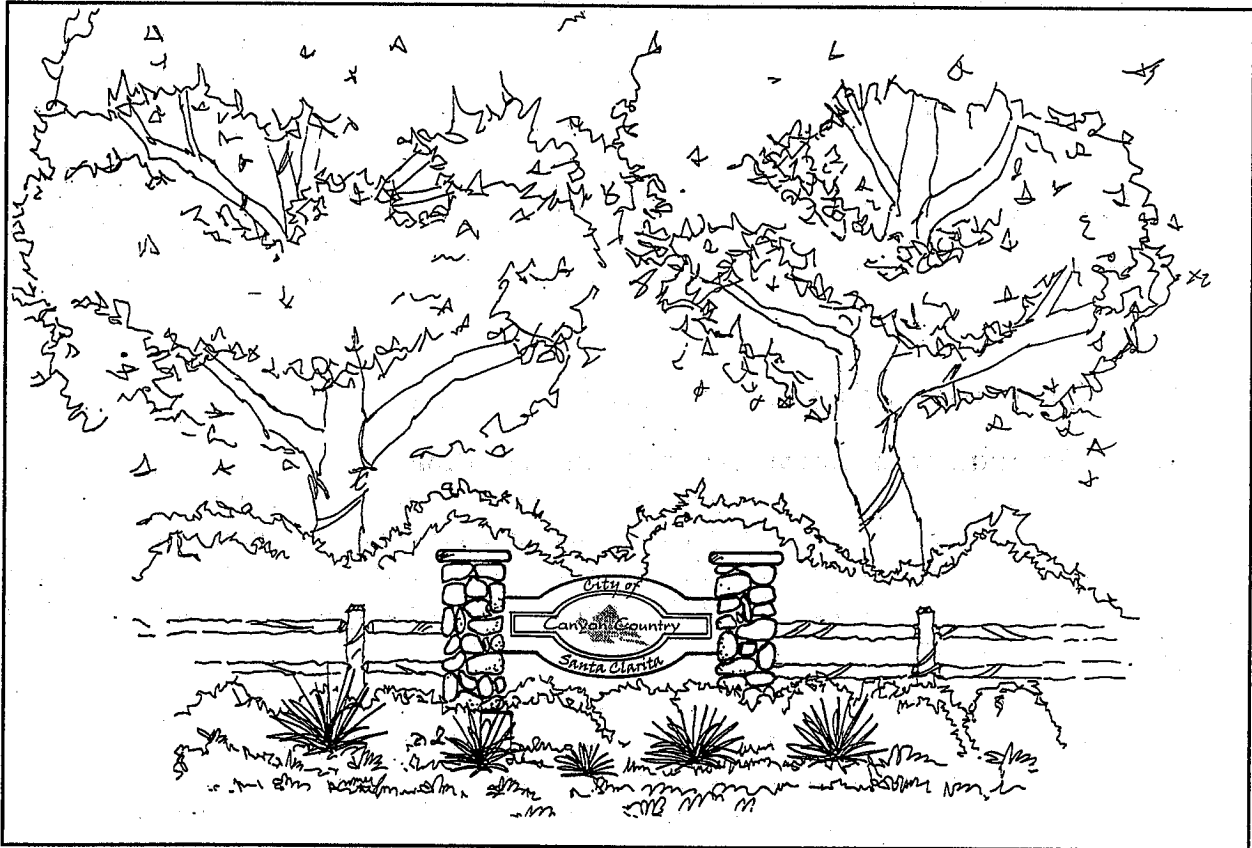
- North to south: Plum Canyon Road to Whites Canyon Road
- North to south (Hwy. 14): Shadow Pines exit onto Soledad Canyon Road
- South to north: Placerita Canyon Road / Sierra Highway
- North to south: Sierra Highway south of Davenport Road and north of Canyon Country little league
- West to east: Soledad Canyon Road / Cordo Drive

2. Community Entrance Design

Community Entrance Character:

The community entrance is one of the best opportunities to reflect its individuality. The character and materials used should uniquely represent the community. The following materials and design styles were identified to describe Canyon Country during a public workshop.

- River rock
- Equestrian elements
- Lodge post fence (long, darker/natural color)
- Oak, Pepper and Cottonwood trees
- Aged metal
- Heavy timber
- Ahwanee (style)
- Greene & Greene and Old Pasadena
- Earth tones
- Natural stone features



Proposed: Canyon Country Community Entry Scene - Pepper and Oak trees, natural lodge pole fence, special plant palette, and a monument sign with river rock, community logo, and City name.

Community Entry Signs:

In the *Citywide Design Guidelines* section, “Entry Monuments” will help meet the goal to unify the City as a whole while the “Community Entry Signs” will help meet the other half of the goal, to embrace the different characters of individual communities. The Canyon Country entry sign reflects the unique natural setting and materials of the area. Community entry signs should have the following characteristics:

- Community entry signs should be located at each of the previously mentioned community entrance locations and should be coordinated with Caltrans or private landowners as appropriate. Specific locations should be determined based on visibility, right-of-way availability, sight distance criteria and minimum square footage requirements.
- Community entry signs should be lighted and landscaped.
- Community entry signs should be an integral part of the gateway scene which includes an entry sign, a fence and plants and trees approved by the Director of Parks, Recreation and Community Services.
- Community entry signs should be well proportioned and accented with landscaping. The scale of the signs should be easily legible from the road. Planting at maturity should not obscure safe vehicular sight lines.
- Community entry signs should incorporate complementary colors, materials and lettering fonts. More than one material should be used in the construction of the sign and should reflect the character of the community. The sign should be constructed for long-term durability and easy maintenance.
- All community entry signs should use the same materials, color scheme and graphic theme in order to make it very clear to visitors and residents alike that they are placed as an effort to unify Canyon Country.
- The community logo should appear on each sign and a community-specific font should be replicated exactly on each Canyon Country entry sign.
- “City of Santa Clarita” should appear on each community entry sign. The same font should be used for the citywide entry monuments.

The following landscaping palette has been created using and enhancing existing plant palettes applied throughout the Canyon Country area. Future planting should utilize this list or plants that are similar and compliment the desired theme.

Community Entry Landscaping:

Trees

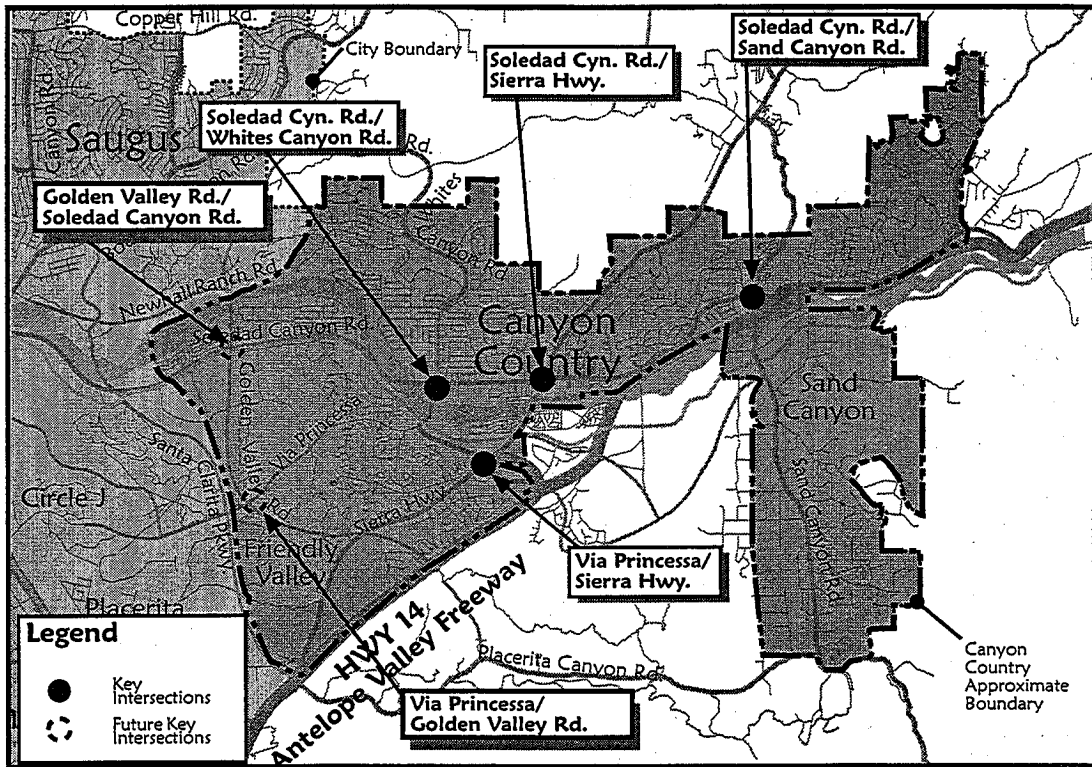
Botanical Name	Common Name
<i>Platanus racemosa</i>	California Sycamore
<i>Quercus lobata</i>	Valley Oak

Shrubs

Botanical Name	Common Name
<i>Arctostaphylos d. 'Howard McMinn'</i>	Manzanita
<i>Ceanothus concha</i>	California Lilac
<i>Garrya elliptica</i>	Coast Silktassel
<i>Heteromeles arbutifolia</i>	Toyon
<i>Mahonia aquifolium</i>	Oregon Grape
<i>Rhus ovata</i>	Sugar Bush
<i>Ribes viburnifolium</i>	Evergreen Currant
<i>Yucca whipplei</i>	Our Lord's Candle

Ground Cover

Botanical Name	Common Name
<i>Arctostaphylos hookeri</i>	Monterey Manzanita
<i>Baccharis pilularis</i>	Dwarf Coyote Brush
<i>Cistus salviifolius</i>	Rock Rose
<i>Mahonia repens</i>	Creeping Mahonia
<i>Rosmarinus o. 'Prostratus'</i>	Dwarf Rosemary



Key Intersections Map

3. Key Intersection Treatment and Locations

There are several intersections within the community of Canyon Country that stand out as opportunities to beautify and claim as unique places for Canyon Country. These intersections are large with multiple lane thoroughfares requiring motorists and pedestrians alike to wait for several minutes and soak in the surrounding environment. Pedestrians have to cross over several lanes of traffic in order to get from place to place. It is the intent of these guidelines to beautify these intersections to the extent possible and create a more safe pedestrian-friendly environment. The following key intersections have been identified as areas that need priority improvement.

Key Intersections

- Soledad Canyon Road / Whites Canyon Road
- Soledad Canyon Road / Sierra Highway
- Soledad Canyon Road / Sand Canyon Road
- Via Princessa / Sierra Hwy.

Future Key Intersections

- Via Princessa / Golden Valley Road
- Golden Valley Road / Soledad Canyon Road

Beautification of the intersections should include:

- Accent paving
- Landscaping and street trees
- Directional signage / reduction of visual clutter
- Lighting and signal pole treatment
- Underground utility poles and wires

The Sierra Highway and Soledad Canyon Road intersection illustrated on the following pages depicts the advantages of implementing some of these beautification concepts.

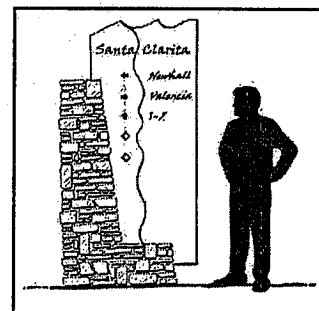
Accent Paving:

The use of accent paving provides an opportunity to both dress up the intersection and incorporate traffic calming measures for pedestrian and bicyclist safety. The change in texture is a visual and intuitive signal to both the motorist and pedestrian of the appropriate boundaries for each. Accent paving contributes greatly to the overall visual appeal of the intersection and is typically located in one or more of the following areas: at corners; on sidewalks; at pedestrian crossings; and in the middle of intersections.

At key intersections, the pedestrian crosswalks should be treated with a durable colored concrete pattern so as to visually cue the oncoming motorist to slow down because of possible pedestrian traffic. Special paving also enhances the pedestrian experience, both in visual appeal and safety. The texture should be smooth enough so that an older person, cyclist and persons with disabilities are not deterred. In addition, a surface that limits noise should be utilized. The paver selected for Canyon Country should match the stamped concrete pattern used in the median. The concrete is colored river rock grey with obsidian and burnt orange speckles, finished with a lithochrome color hardener and shaped like river rock laid in a four-to-six inch pattern.

Signage:

The directional sign described in the *Citywide Signage Program* section of the *Citywide Design Guidelines* should be placed at key intersections in order to help motorists determine the direction of the adjacent communities. It is important that existing directional signage be removed to reduce the amount of visual clutter. In addition to directional signage, the removal or replacement of commercial/business signage and billboard/advertisement signs should be given first priority at key intersections.



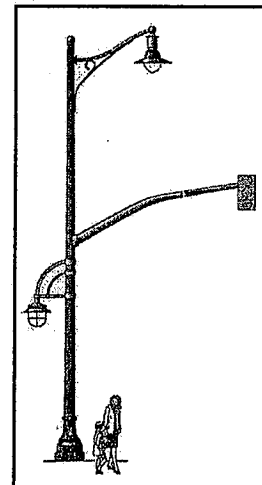
Example of Directional Signage

Landscaping Treatments:

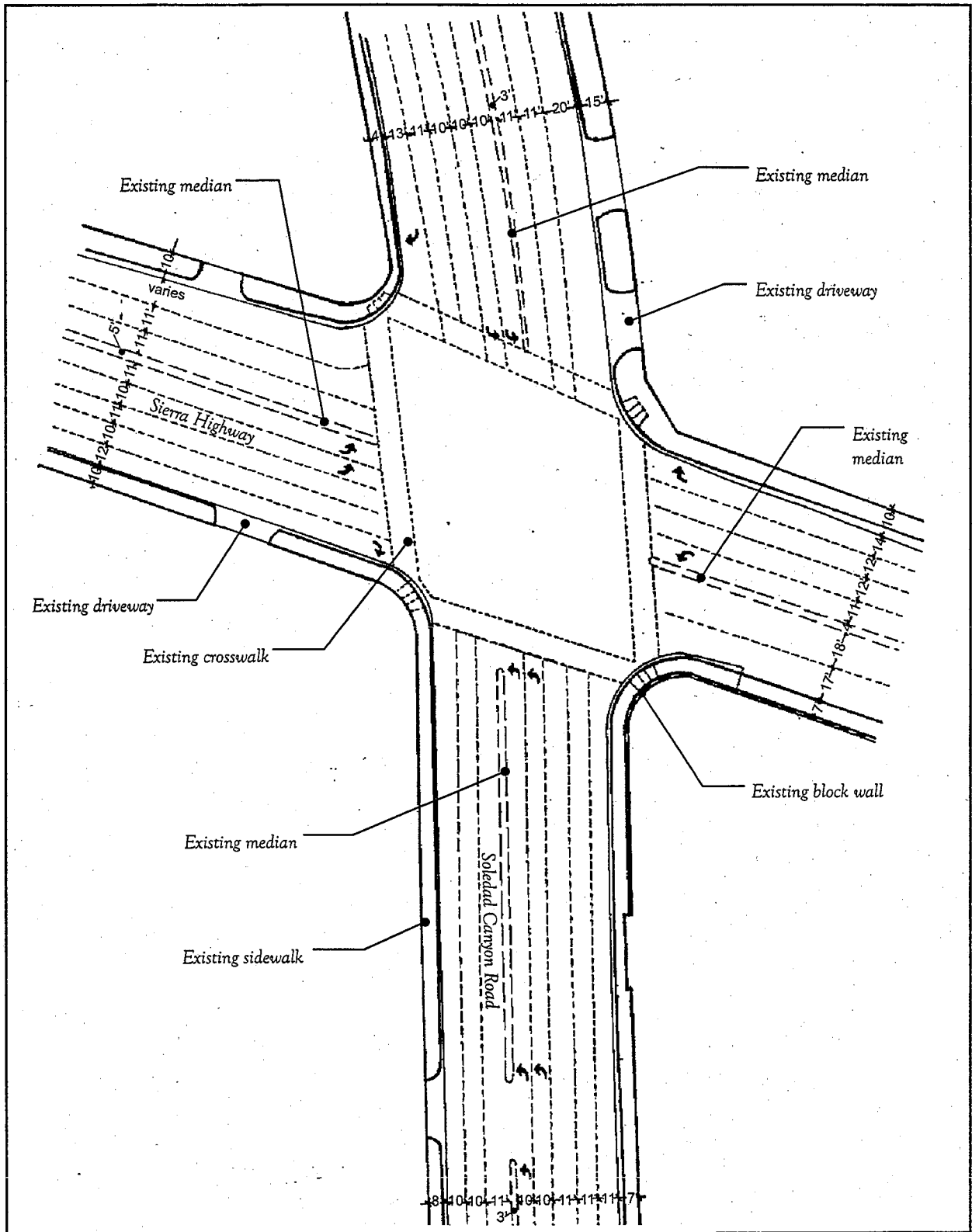
Additional vegetation located at key intersections will enhance the experience of the motorist and pedestrian. Landscaping, low walls and berms should be used to screen large expanses of paving (parking lots, service station lots, etc.) on private property. Medians should be widened to a minimum of six feet where feasible to allow for high branching trees to be planted. Trees with four-foot tree grates should be planted along the sidewalk near the intersection. The trees planted in the medians and in the parkways should correspond to those selected for a given community and primary corridor, respectively.

Lighting:

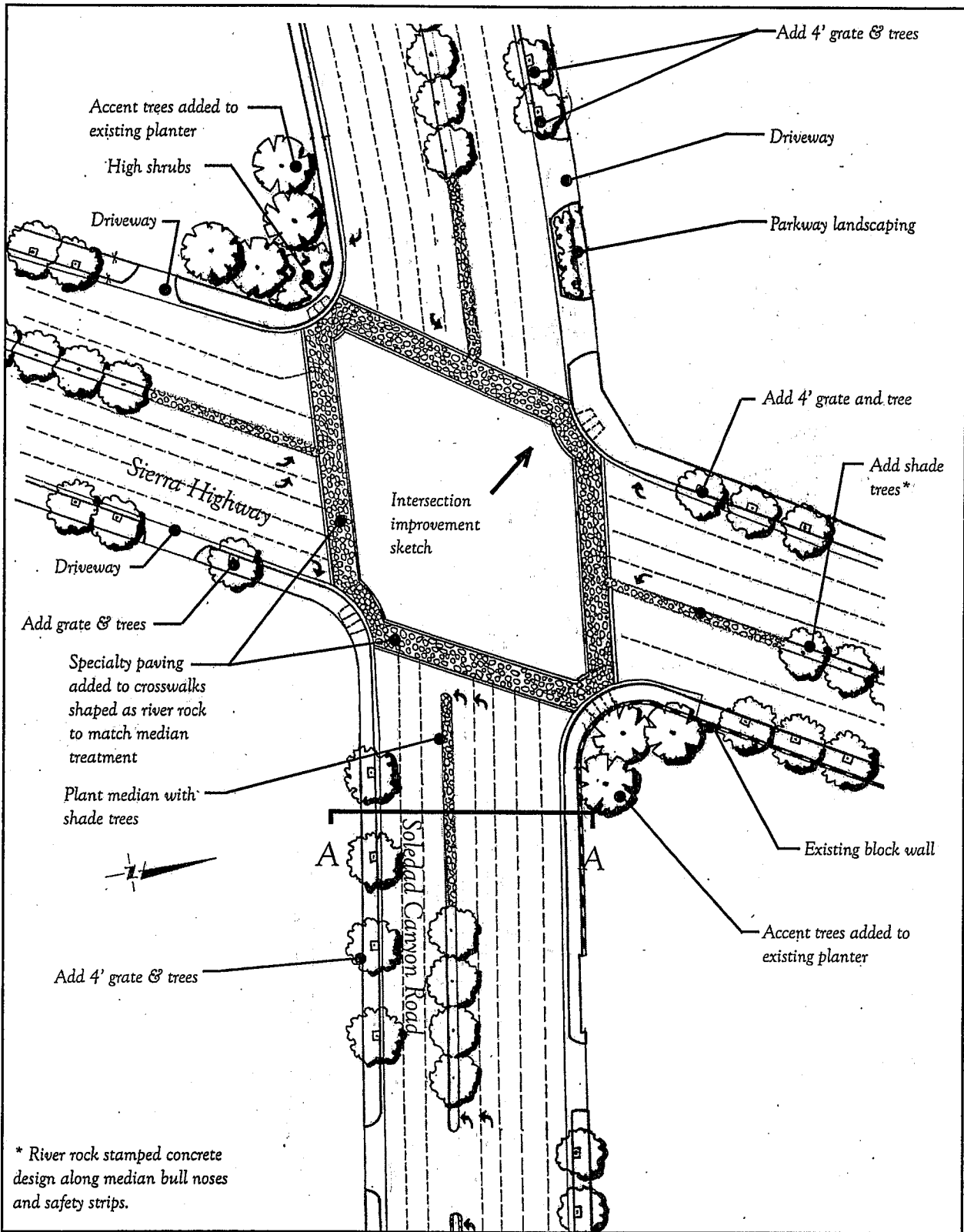
The implementation of a uniquely designed street light and signal poles at key intersections can enhance the experience of the pedestrian and motorist. A minor improvement involving the addition of an ornamental base and fixture to the existing pole can greatly improve its aesthetic appeal. All specially designed street lights and signal poles shall be reviewed and approved by the appropriate agencies.



Example of Specially Designed Street Light and Signal Pole Enhancements



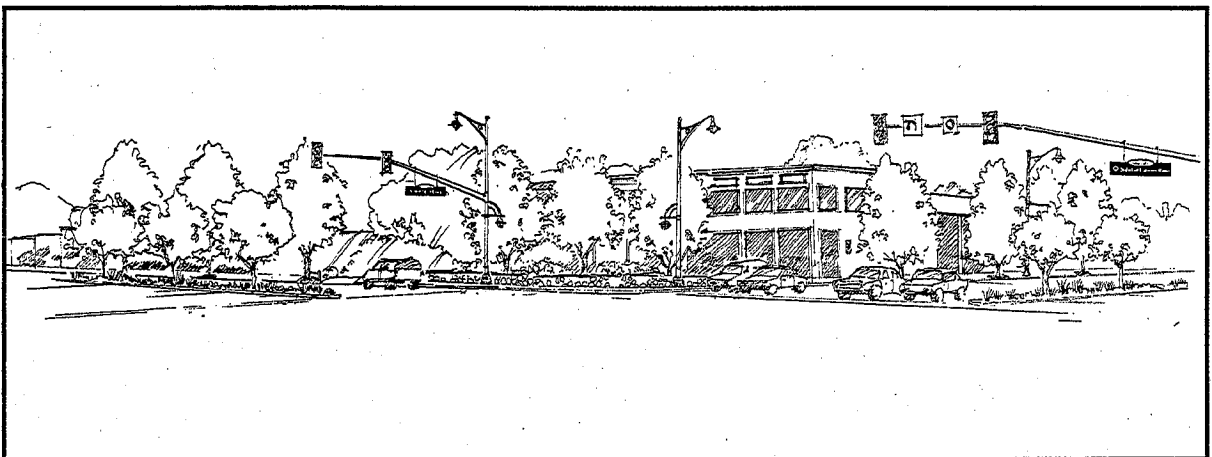
Before: Sierra Highway and Soledad Canyon Road Intersection



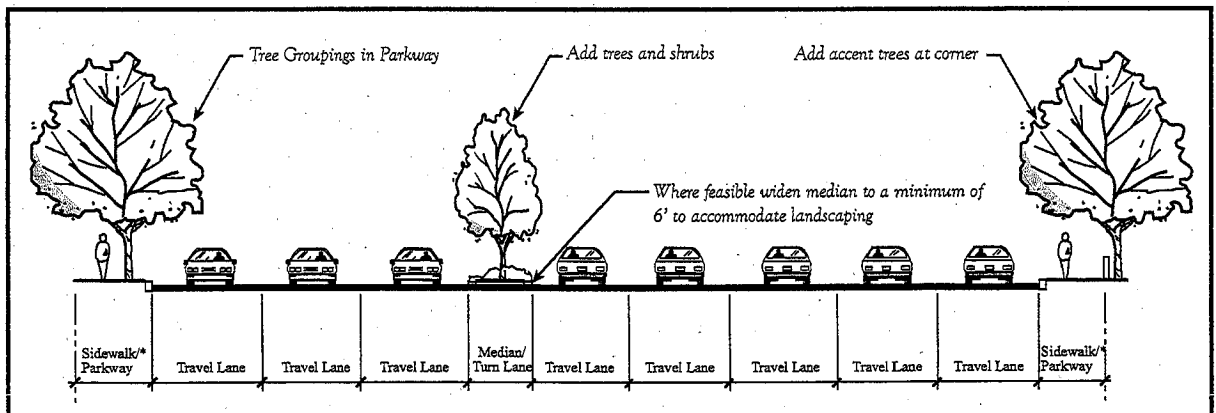
After: Sierra Highway and Soledad Canyon Road Intersection



Before: Sierra Highway and Soledad Canyon Road Intersection



After: Sierra Highway and Soledad Canyon Road Intersection With Improvements



Section A-A: Soledad Canyon Road Intersection With Improvements

D. Other Beautification Ideas

This section addresses the beautification needs identified by participants from various workshops and provides suggestions for additional ways in which a community might encourage beautification efforts.

1. Specific Beautification Needs

The following beautification needs are not directly addressed by the Beautification Master Plan, but are areas that community members have identified as issues needing improvement.

- Vacant lots should be screened with low walls, fences and /or landscaping
- Improve the general commercial corridor appearance along Soledad Canyon Road
- Raise the level of design and quality of new projects

2. Private Property Treatment

To assist city beautification efforts within the street right-of-way, private property owners should strive to accomplish the following:

- Reduce sign clutter
- Landscape parking lots and building frontage
- Screen unsightly storage and trash areas
- Coordinate wall/fence treatment between properties
- Maintain vacant lots free of trash and weeds
- Seek to upgrade building facades consistent with design image
- Plant trees to add shade and soften urban look
- Use Canyon Country fence (post and rail) with planting along roads at vacant parcels

3. Getting Involved In The Process

Community groups should make themselves aware of the City design review and approval process. Likewise, the City should notice a member of the organization of upcoming meetings and proposed projects. This established communication will help to ensure that projects are approved that residents and City staff are proud of. In addition, community groups might take on banner projects such as organizing and encouraging private property owners to improve their building facades or plant vines in front of unsightly walls and fences.

NEWHALL

Newhall

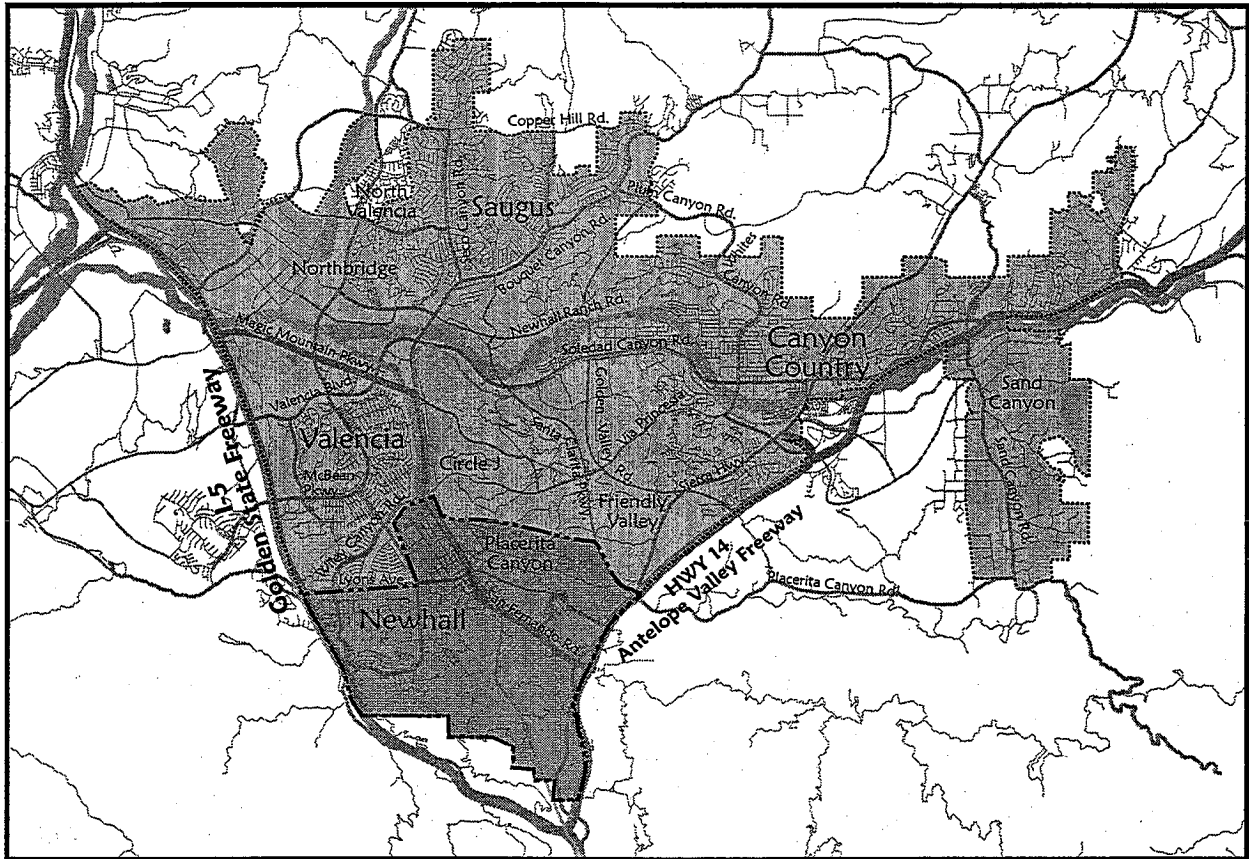
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III. Newhall Community Design Guidelines

A. Introduction

The following beautification guidelines contain specific recommendations for the community of Newhall. Where there is overlap with the *Citywide Design Guidelines*, cross references are made. Though there is not a clear boundary for Newhall, the map below delineates the general area. These guidelines focus on areas within the public right-of-way in Newhall.



Community of Newhall Boundary

1. Community Identity

Newhall is a community rich in history. Evolving from an Indian trading center to an important destination in the quest for gold and then oil, it soon became a railroad flag stop of general stores, saloons and churches. As the film industry grew, Newhall became the home of early western silent screen movies. These strong roots are still very important to the residents of Newhall. The passion for history, love of the old film stars such as William S. Hart, the Oak of the Golden Dream (Jose Francisco de Garcia Lopez reached down after a nap and found gold), the distinction of Vasquez Canyon Rocks and the Saint Francis Dam break that marked the second largest natural disaster in California’s history, help to identify the essence of Newhall.

The following characteristics were used to describe Newhall during a public workshop.

- Vasquez Canyon Rocks – jagged rock
- Western movies
- William S. Hart
- Passion for history
- Cowboys
- Melody Ranch – filming
- Oak of the Golden Dream
- Oil fields (Mentryville)
- Last black bear shot was in Newhall
- Rich cultural heritage
- Saint Francis Dam

2. Beautification Needs

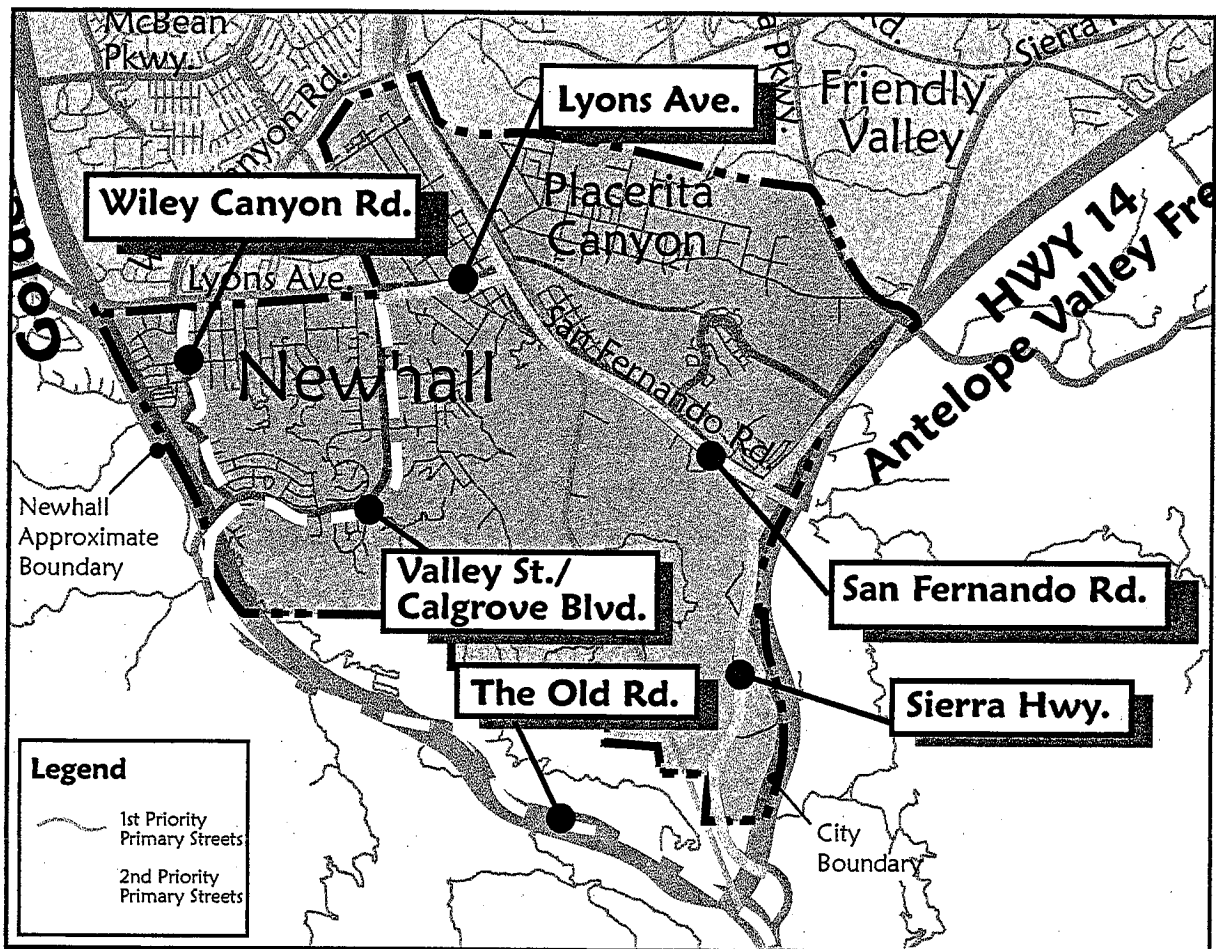
Recognizing the need for beautification in their community, Newhall has organized a number of volunteer groups that have committed many hours of hard work to help define community beautification goals and identify specific needs. These groups have contributed significantly to the contents of these guidelines.

The following list of beautification needs was generated at a public workshop conducted with members of the community, members of the Newhall volunteer organizations and Santa Clarita planning staff.

- More trails should be created in Newhall
- A continual effort should be achieved to remove advertisement billboards.
- Screening should be utilized in order to hide mobile home parks and storage yards.
- Key intersections should be designed consistently with crosswalks, landscaping, amenities and lighting.
- Street parkways and medians should be planted.
- The amount of visual clutter including signs, overhead utilities and vehicular access points should be reduced.
- A unique logo to help unify and distinguish Newhall should be developed.
- Community entry signage should be designed with a logo image, colors and font that represents Newhall.
- Community identity should be strengthened, but not at the expense of Citywide unification.
- Bus shelters should improved and sheltered. Advertising benches should be replaced. (Refer to the *Citywide Public Transportation* section of this document.)

Based on this community feedback, the following beautification guidelines were drafted to address the following topics. The guidelines address a range of design improvements that can lead to a more cohesive and unified look for Newhall.

- | | | |
|----------------------|---------------------------|-------------------------------|
| 1) Primary corridors | 4) Street edge treatments | 7) Main intersections |
| 2) Signs | 5) Fences | 8) Other beautification ideas |
| 3) Medians | 6) Community entries | |



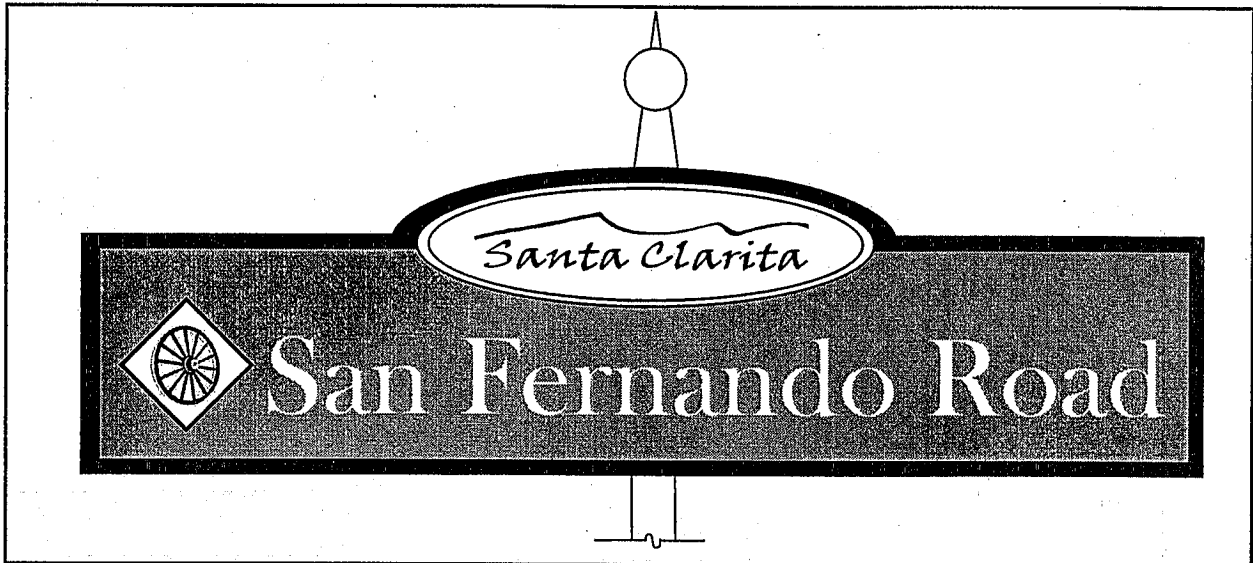
Newhall Primary Streets

B. Treatment of Primary Corridors

The primary corridors that extend through a given community act as the most visible and consequently the strongest unifying element within the community. Each community within the City of Santa Clarita should be clearly identifiable with distinct primary corridors that help unify it. The implementation of a community logo on each street sign, a consistent median and corridor edge treatment, a unique fence type and landscaping improvements bring together the image of a cohesive community. One of the goals of the Beautification Master Plan is to set forth long-range guidelines for improvements, enhancements and beautification of such corridors so that implementation occurs in a direction that creates a clear and easily recognizable individual community. The following streets have been designated as primary corridors within Newhall.

- First Priority Primary Streets: Sierra Highway, San Fernando Road and Lyons Avenue
- Second Priority Primary Streets: Valley Street / Calgrove Boulevard, The Old Road and Wiley Canyon Road

1. Signs and Logo



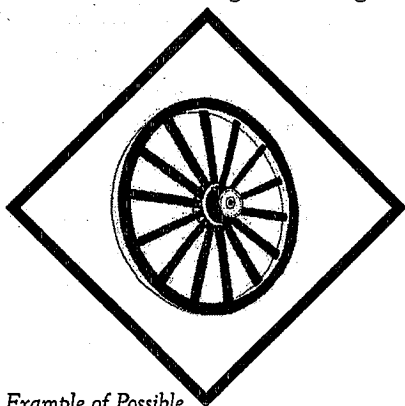
Example of a Street Sign Designed to Incorporate a Newhall Logo

Street Sign:

The most prevalent and obvious component of the primary corridor elements is the street sign treatment. It is here that a clear and distinct demarcation occurs for each individual community. Every street sign within the community of Newhall should contain a community logo. This logo provides a recognizable sense of repetition that clearly enables a motorist, bicyclist or pedestrian to know where they are and if they are still within the community limits. The logo will be placed within a diamond on the left side of the sign and each diamond will be a specific color for each community. This simple and quickly recognizable feature will provide a highly visible statement as to the community that an individual is in. Refer to the *Citywide Signage Program* for further information.

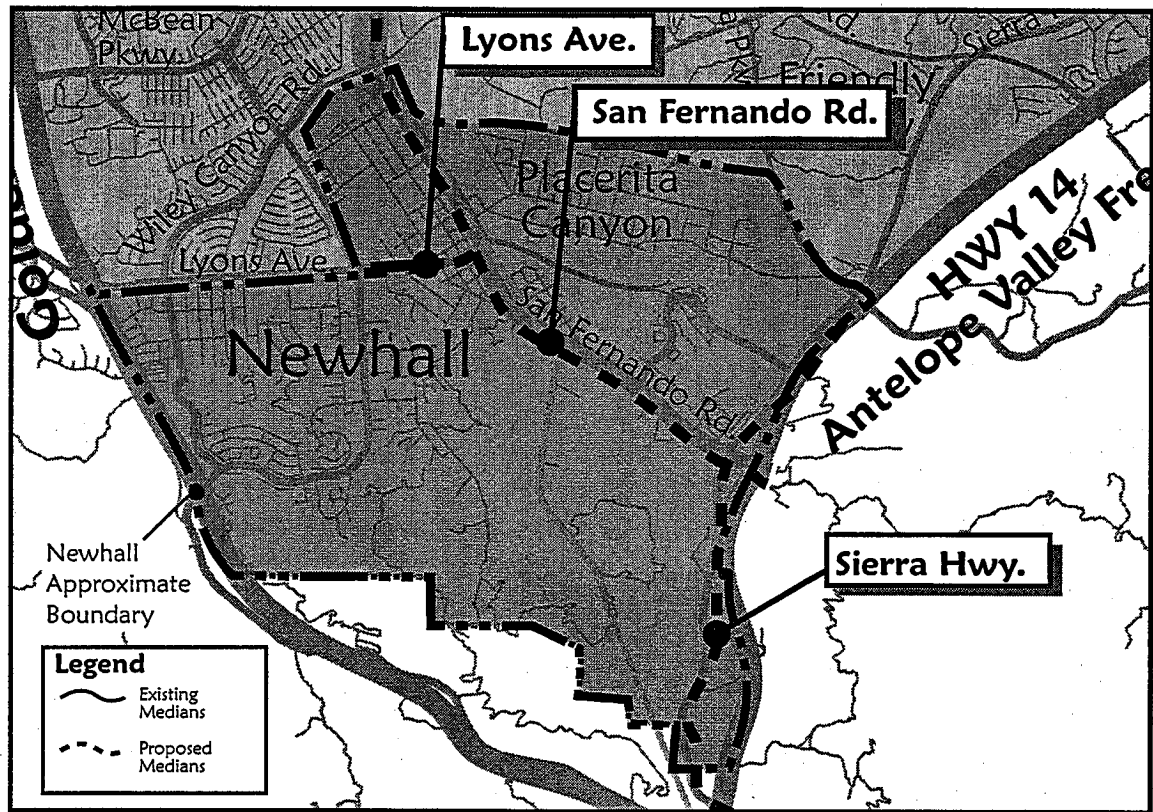
Newhall Community Logo:

Through the interview and public outreach process, participants generated a list of ideas for a Newhall logo. The logo should be unique to Newhall and not something that could represent any number of towns in the region. It should be a logo or symbol that is timeless, avoiding trendy images, colors or fonts.



Example of Possible Newhall Community Logo

- Wagon wheel
- William S. Hart with cowboy hat
- Western Walk of Fame
- Downtown street light
- Oil derrick
- Profile of cowboy with movie reel
- Oak tree
- Cowboy hat
- Railroad
- Melody Ranch
- Bell tower at the new Newhall Metrolink Station



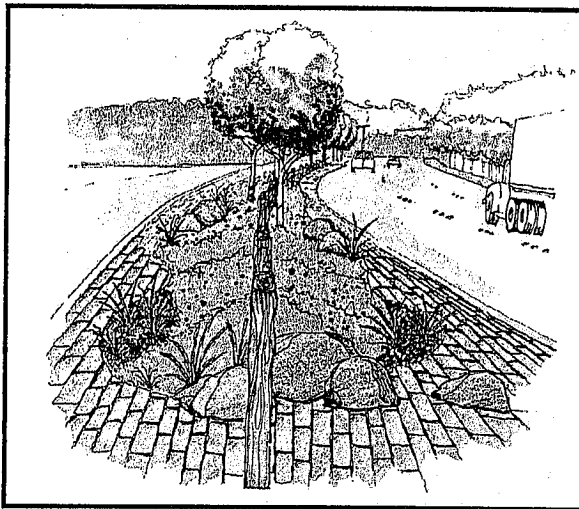
Newhall Medians Map

2. Median Treatment

One intent of the Beautification Master Plan to build on and extend Santa Clarita’s existing network of roadway medians into one, cohesive citywide plan that identifies each community. Each community will have a planting theme to promote the area. The plant palette selection that will be used in the beautification and implementation of the medians will continue to enhance the adjacent landscape. The materials shall be consistent within Newhall and act as a clear indicator of the difference between communities. New raised and planted medians should be considered a long-term objective for community beautification. Along with providing aesthetic appeal and consistency, the proposed medians will also control vehicle circulation, movements and serve to calm traffic. The medians will also provide a place for roadway approach street signs and directional signage at key intersections as part of the citywide signage program discussed in the *Citywide Design Guidelines*. Within Newhall and vicinity, the selected median treatment will be applied to the following roads at the locations indicated on the figure above.

- **Sierra Highway** - from Santa Clarita Parkway south to Highway 14
- **San Fernando Road** - from Wiley Canyon Road south to Highway 14
- **Lyons Avenue** - from Orchard Village Road east to San Fernando Road

Currently, median maintenance is funded by either City General Fund or privately-funded Landscape Maintenance Districts. These different funding sources provide opportunities for different median design concepts. Both designs incorporate a similar plant palette that will help to enhance and unify the community. A drought tolerant design offers a lower maintenance



Example of Newhall Median Treatment Concept



Interlocking Concrete Paver Safety Strip

solution with plants that have an ability to thrive in tough conditions and require less water. A wide variety of plants have been selected that can add beauty and diversity to any landscape. A combination of crescent shaped landscaped pockets and stamped concrete paving should continue to be used in areas that are funded by the City General Fund.

The median treatment concept that has been selected for Newhall is a drought tolerant landscaping design that integrates large Vasquez rocks and a drip irrigation system. An undulating safety curb of interlocking concrete pavers surrounds the landscaping. The pavers are a blend of greys, reds, and brown colors and are shaped like rectangular

cobble or brick. The natural split rail fence described in the following pages could be constructed to run along the center of the median to reinforce the western theme. The median design must comply with all public safety requirements.

The following landscaping palette has been created using and enhancing existing plant palettes applied throughout the area. Future planting should utilize this list or plants that are similar and compliment the desired theme.

Median Landscaping:

Trees types which are being used

Botanical Name

Koelreuteria bipinnata

Plantanus acerifolia

Pinus eldarica

Pyrus species

Rhus lancea

Robinia pseudoacacia

Liquidambar 'Rotundiloba'

Common Name

Chinese Flame Tree

London Plane Tree

Afghan Pine

Ornamental Pear

African Sumac

Black Locust

American Sweet Gum

Shrubs which are being used

Botanical Name

Hemerocallis

Muhlenbergia rigens

Phormium tenax

Pittosporum tobira 'Wheeler's Dwarf'

Rhaphiolepis umbellata 'Minor'

Common Name

Daylily

Deer Grass

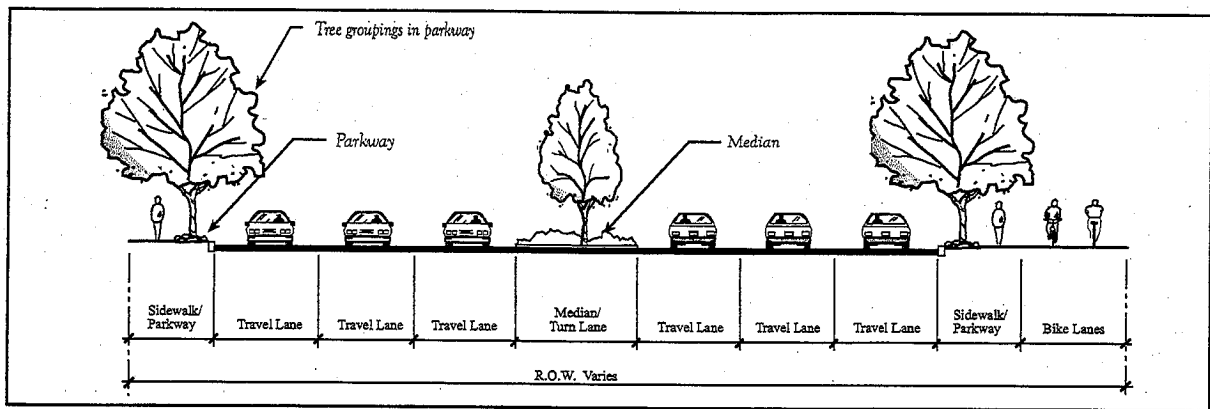
New Zealand Phlax

Tobira

Median Landscaping Continued:

Ground covers which are being used

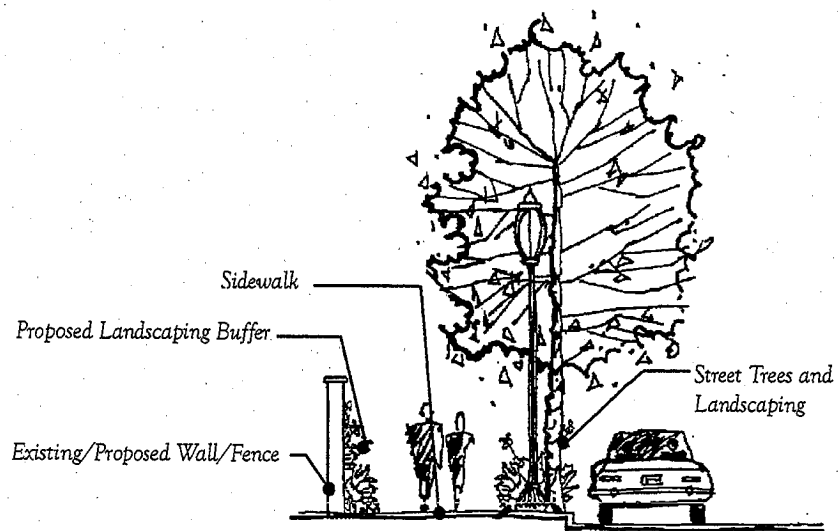
Botanical Name	Common Name
<i>Cistus crispus</i>	Rockrose
<i>Cotoneaster dammeri</i>	Bearberry Cotoneaster
<i>Myoporum parvifolium</i> 'Putah Creek'	-----
<i>Rosmarinus officinalis</i>	Rosemary
<i>Scaevola</i> 'Maui Clusters'	-----



Example of Roadway Section

3. Street Edge Treatment

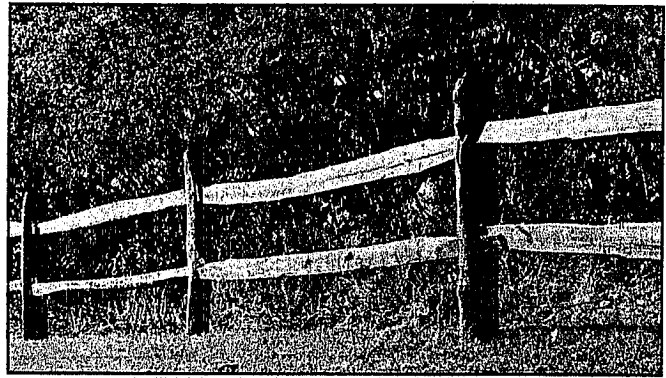
The sides of the primary corridors are just as important as the median treatment. Well landscaped parkways provide pleasant driving and walking environments while creating a sense of place. A planting palette for such parkways is included in the *Citywide Landscaping Palettes* section of this document.



Example of Street Edge Treatment

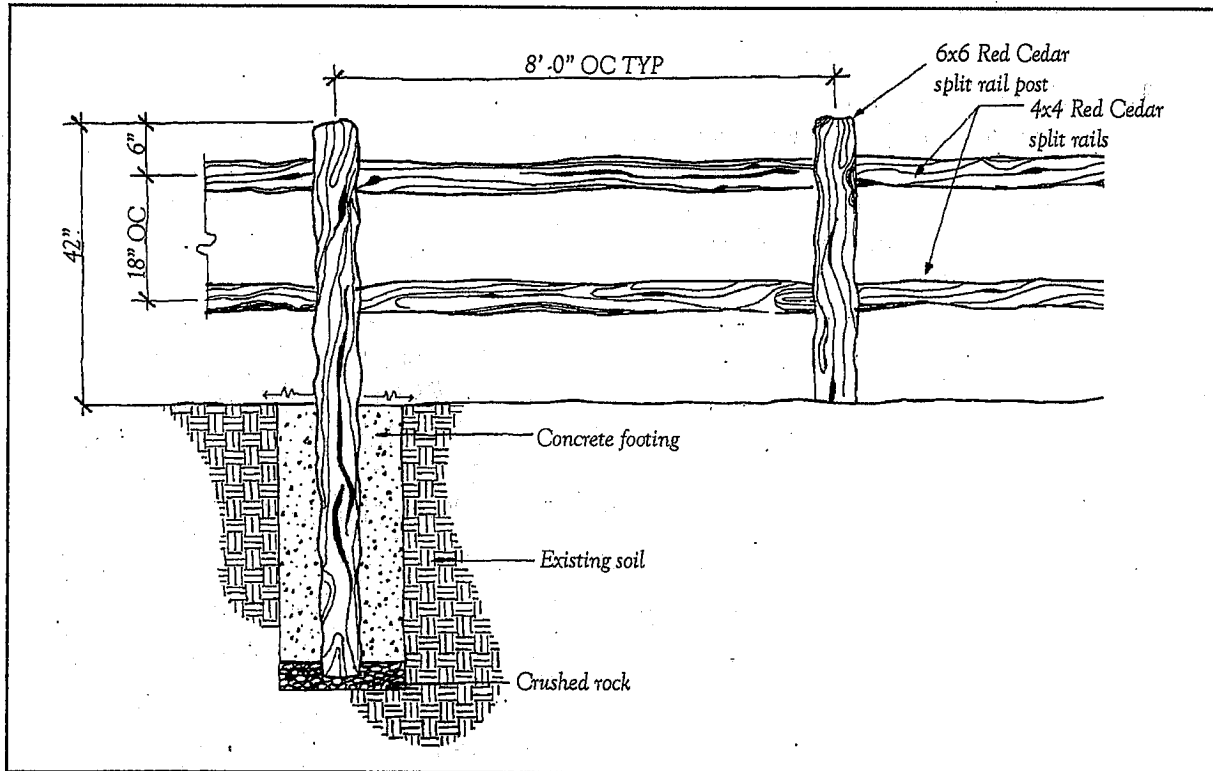
Fences:

Fences should be used to unify the City of Santa Clarita. They provide accent and help define a space and can be used to buffer parking lots adjacent to the roadway. In addition, fences used at vacant lots can “bridge” large void spaces and provide unity along the street scene. Each community will incorporate its own fencing type at intersections and along primary corridors where appropriate.

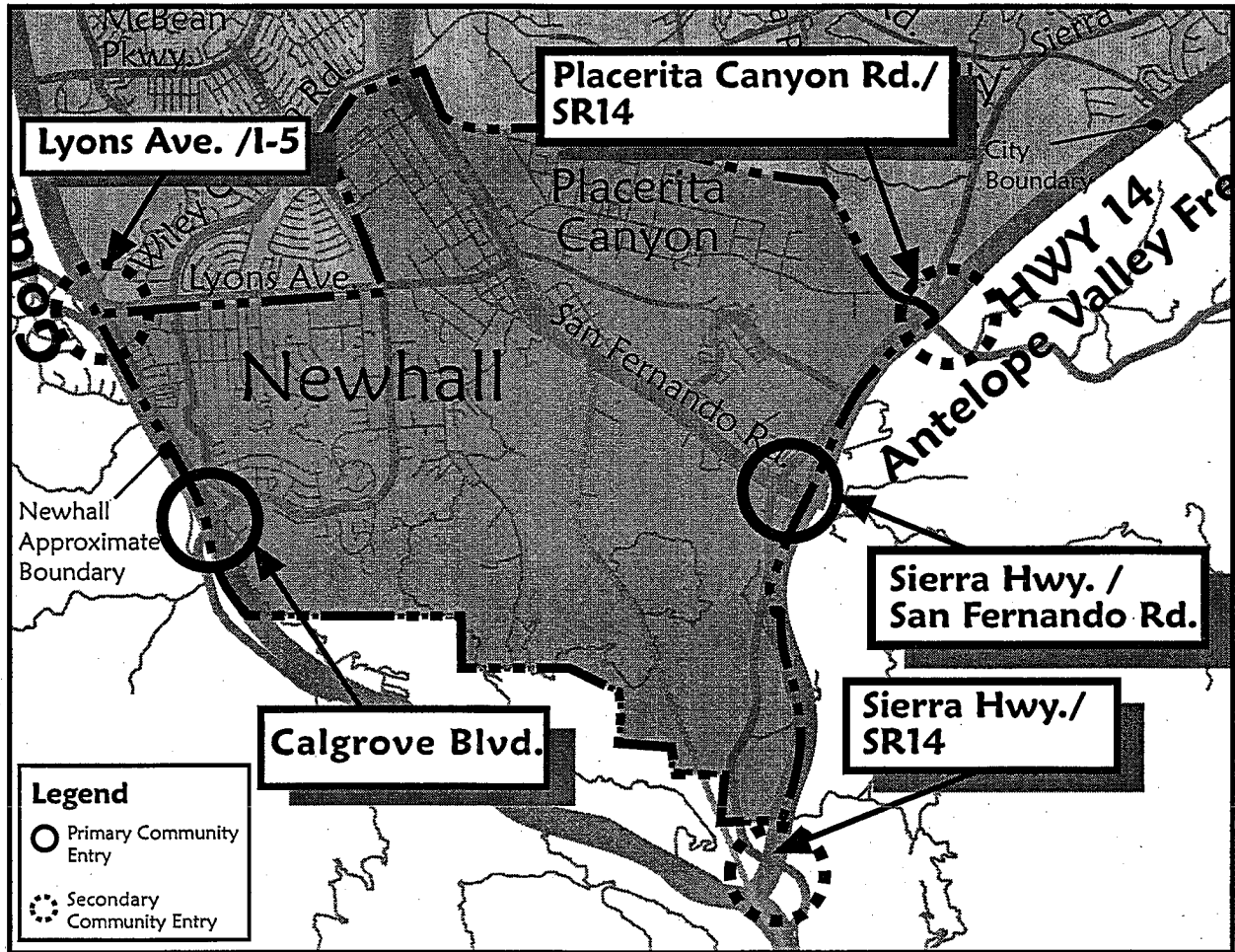


Example of Natural Split Rail Fence

The natural split rail fence selected for Newhall blends nicely with the landscaping and reflects the rustic and historic character of the community. The following materials should be used for the split rail fence. Red cedar or precast concrete 4x4 rails should be set into predrilled 6x6 red cedar posts placed 8' on center. Footing for posts should be set in concrete for maximum stability and placed over crushed rock to allow water to drain through and minimize post decay.



Red Cedar Split Rail and Post Detail



Newhall Entrances Map

C. Community Entrances and Intersections

1. Community Entrance Treatment and Locations

The entrance to each individual community should have a unique and easily identifiable “scene” that includes specific landscaping, a fence and a unique community-oriented sign. The entrances should give visitors and residents a feeling of what to expect within the community and be welcoming without being overstated. The following locations have been selected for community entrance treatments.

Primary Community Entries:

- Lyons Avenue and I-5
- San Fernando Road and SR14

Secondary Community Entries:

- Sierra Highway and SR14
- Placerita Canyon Road and SR14
- Calgrove Boulevard and I-5

Future Primary Community Entries:

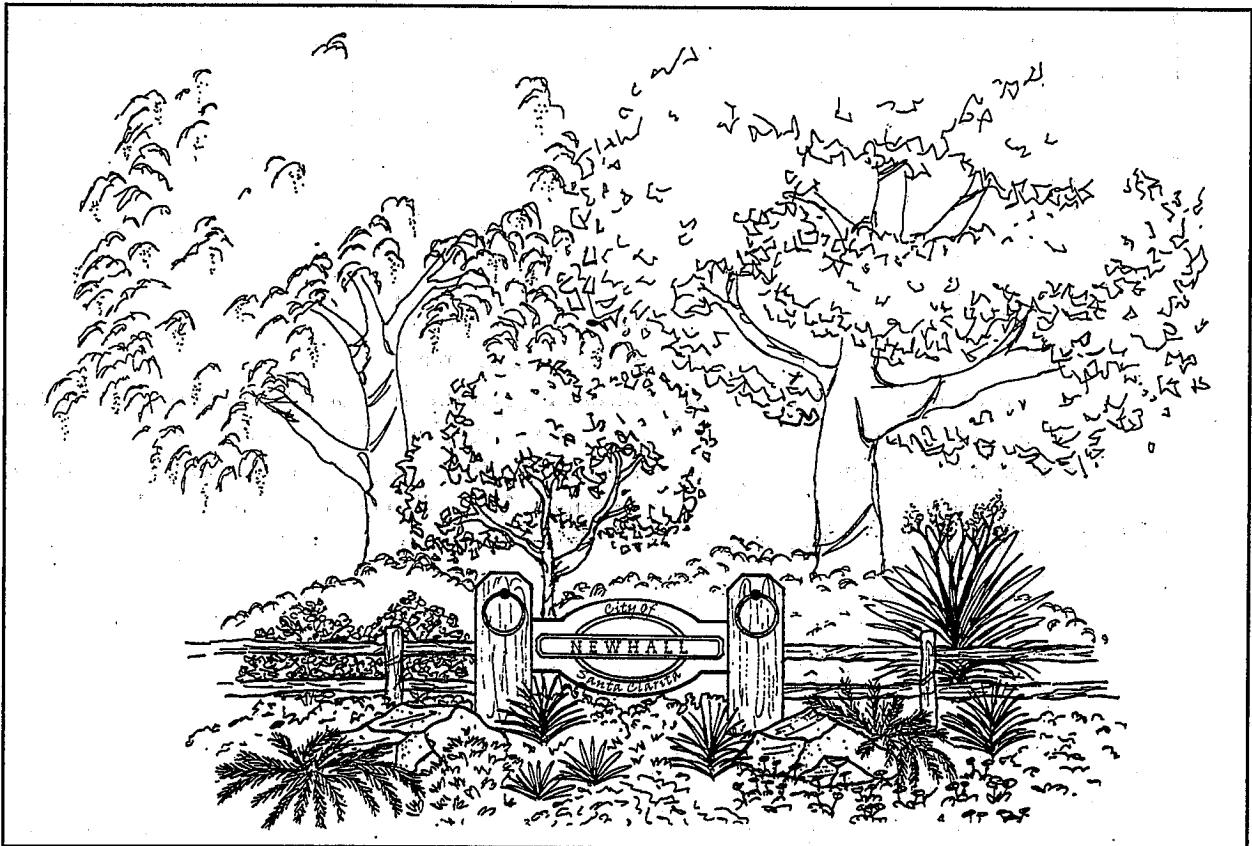
- Santa Clarita Parkway and SR14

2. Community Entrance Design

Community Entrance Character:

The community entry is one of the best opportunities to reflect its individuality. The character and materials used should be a unique representation of the community. The following materials and design styles to reflect the character of Newhall were identified during a public workshop.

- Vasquez Rock is prevalent in the area and could be used in the entrance design
- Lettering should be reflective of the historic 18th Century style
- Letters should be cut out or barn red blacksmith design
- Colored rocks - orange or burnt orange - to reflect the natural landscape



Proposed: Newhall Entry Scene: Western Redbud, Coast Live Oak, and California Pepper trees, natural split rail fence, a special plant palette, and a community oriented sign with community logo and City name.

Community Entry Signs:

In the *Citywide Design Guidelines* section, “Entry Monuments” will help meet the goal to unify the City as a whole while the “Community Entry Signs” will help meet the other half of the goal, to embrace the different characters of individual communities. The Newhall entry sign reflects the unique natural setting and materials of the area. Community entry signs should have the following characteristics:

- Community entry signs should be located at each of the previously mentioned community entrance locations and should be coordinated with Caltrans or private landowners as appropriate. Specific locations should be determined based on visibility, right-of-way availability, sight distance criteria and minimum square footage requirements.
- Community entry signs should be lighted and landscaped.
- Community entry signs should be an integral part of the gateway scene which includes an entry sign, a fence and plants and trees approved by the Director of Parks, Recreation and Community Services.
- Community entry signs should be well proportioned and accented with landscaping. The scale of the signs should be easily legible from the road. Planting at maturity must not obscure safe vehicular sight lines.
- Community entry signs should incorporate complementary colors, materials and lettering fonts. More than one material should be used in the construction of the sign and should reflect the character of the community. The sign should be constructed for long-term durability and easy maintenance.
- All community entry signs should use the same materials, color scheme and graphic theme in order to make it very clear to visitors and residents alike that they are placed as an effort to unify Newhall.
- The community logo should appear on each sign and a community-specific font should be replicated exactly on each Newhall entry sign.
- “City of Santa Clarita” should appear on each community entry sign. The same font should be used for the citywide entry monuments.

The following landscaping palette has been created using and enhancing existing plant palettes applied throughout the Newhall area. Future planting should utilize this list or plants that are similar and compliment the desired theme.

Community Entry Landscaping

Trees

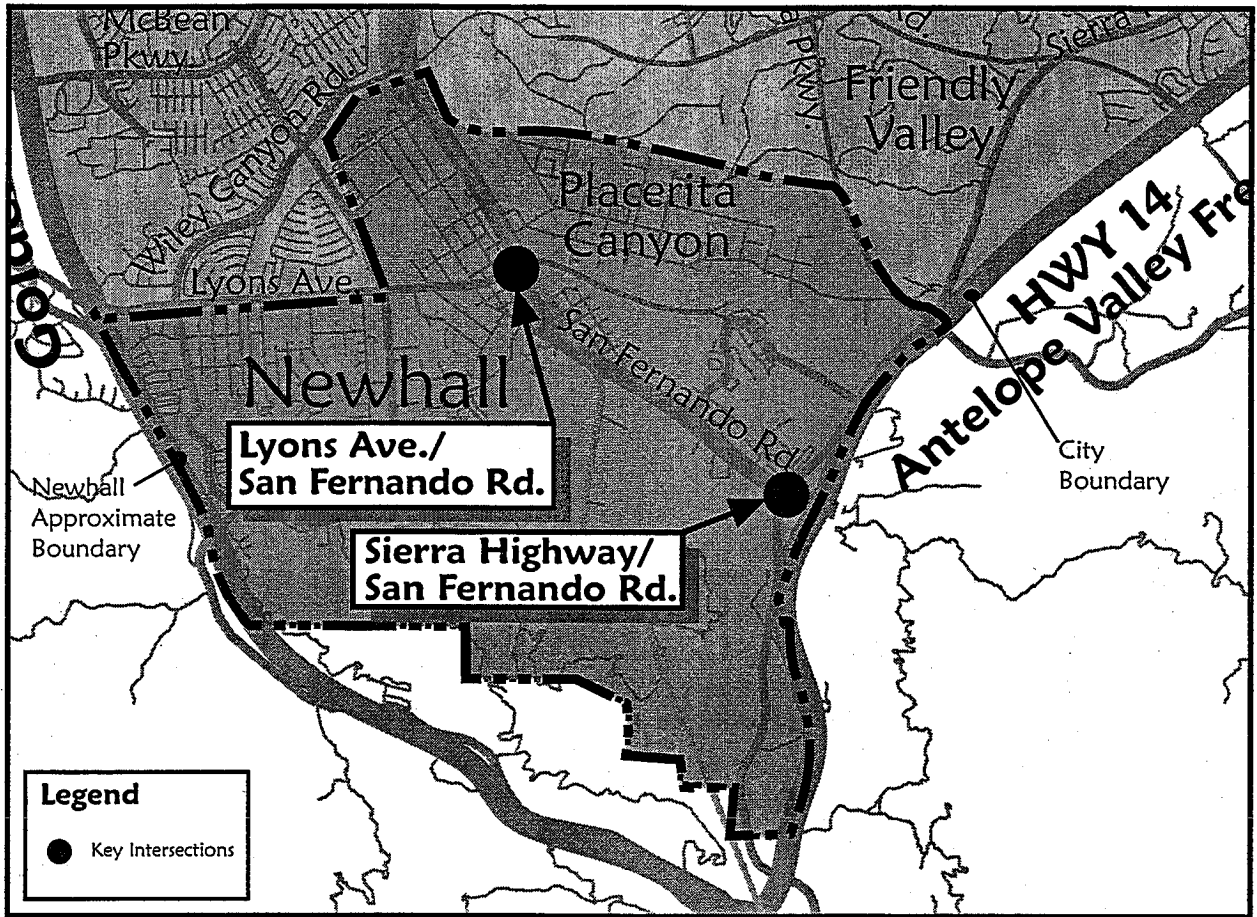
Botanical Name	Common Name
<i>Koelreuteria bipinnata</i>	Chinese Flame Tree
<i>Pinus eldarica</i>	Afghan Pine
<i>Pyrus species</i>	Ornamental Pear
<i>Quercus lobata</i>	Valley Oak
<i>Quercus agrifolia</i>	Coast Live Oak
<i>Rhus lancea</i>	African Sumac
<i>Schinus molle</i>	California Pepper
<i>Liquidambar 'Rotundiloba'</i>	American Sweet Gum

Shrubs

Botanical Name	Common Name
<i>Artemisia 'Powis Castle'</i>	Artemisia
<i>Grevillea rosmarinifolia</i>	Rosemary Grevillea
<i>Hemerocallis hybrids</i>	Daylily
<i>Muhlenbergia rigens</i>	Deer Grass
<i>Phormium tenax</i>	New Zealand Flax
<i>Pittosporum tobira 'Wheeler's Dwarf'</i>	Tobira
<i>Rosmarinus officinalis</i>	Rosemary
<i>Rhaphiolepis umbellata 'Minor'</i>	-----

Ground Cover

Botanical Name	Common Name
<i>Cerastium tomentosum</i>	Snow-in-Summer
<i>Convolvulus mauritanicus</i>	Ground Morning Glory
<i>Coprosma kirkii</i>	-----
<i>Cotoneaster dammeri 'Lowfast'</i>	Bearberry Cotoneaster
<i>Myoporum p. 'Putah Creek'</i>	-----
<i>Scaevola 'Mauve Clusters'</i>	-----



Key Intersections Map

3. Key Intersection Treatment and Locations

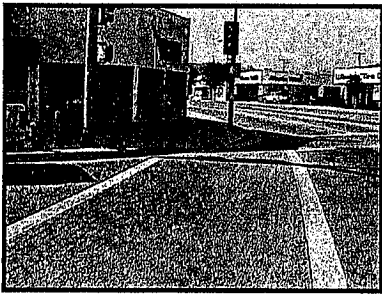
There are several intersections within the community of Newhall that stand out as opportunities to beautify and claim as unique places for Newhall. These intersections are large with multiple lane thoroughfares requiring motorists and pedestrians alike to wait for several minutes and soak in the surrounding environment. Pedestrians oftentimes have to cross over several lanes of traffic in order to travel from place to place. It is the intent of these guidelines to beautify these intersections to the extent possible and create a more pedestrian-friendly environment. The following key intersections have been identified as areas that need priority improvement.

- San Fernando Road and Lyons Avenue
- San Fernando Road and Sierra Highway

Beautification of the intersections should include:

- Accent paving
- Landscaping and street trees
- Directional signage / reduction of visual clutter
- Lighting and signal pole treatment
- Underground utility poles and wires

The San Fernando Road and Sierra Highway intersection illustrated on the following pages depicts the advantages of implementing some of these beautification concepts.

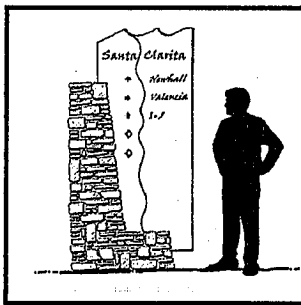


Example of Accent Paving

Accent Paving:

The use of accent paving provides an opportunity to both dress up the intersection and incorporate traffic calming measures for pedestrian and bicyclist safety. The change in texture is a visual and intuitive signal to both the motorist and pedestrian of the appropriate boundaries for each. Accent paving contributes greatly to the overall visual appeal of the intersection and is typically located in one or more of the following areas: at corners; on sidewalks; at pedestrian crossings; and in the middle of intersections.

At key intersections, the pedestrian crosswalks should be treated with a durable colored concrete pattern so as to visually cue the oncoming motorist to slow down because of possible pedestrian traffic. Special paving also enhances the pedestrian experience, both in visual appeal and safety. The texture should be smooth enough so that an older person, cyclist and persons with disabilities are not deterred. In addition, a surface that limits noise should be utilized. Stamped concrete in a red brick pattern should be used as the accent paving throughout Newhall. This will match the existing crosswalks in the downtown area. The special paving can be applied across the entire crosswalk, or just used as a border.



Example of Directional Signage

Signage:

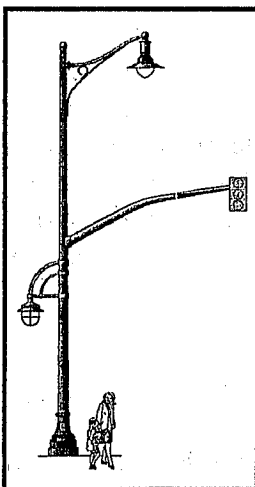
The directional sign described in the Citywide Signage Program section of the *Citywide Design Guidelines* should be placed at key intersections in order to help motorists determine the direction of the adjacent communities. It is important that existing directional signage be removed to reduce the amount of visual clutter. In addition to directional signage, the removal or replacement of commercial/business signage and billboard/advertisement signs should be given first priority at key intersections.

Landscaping Treatments:

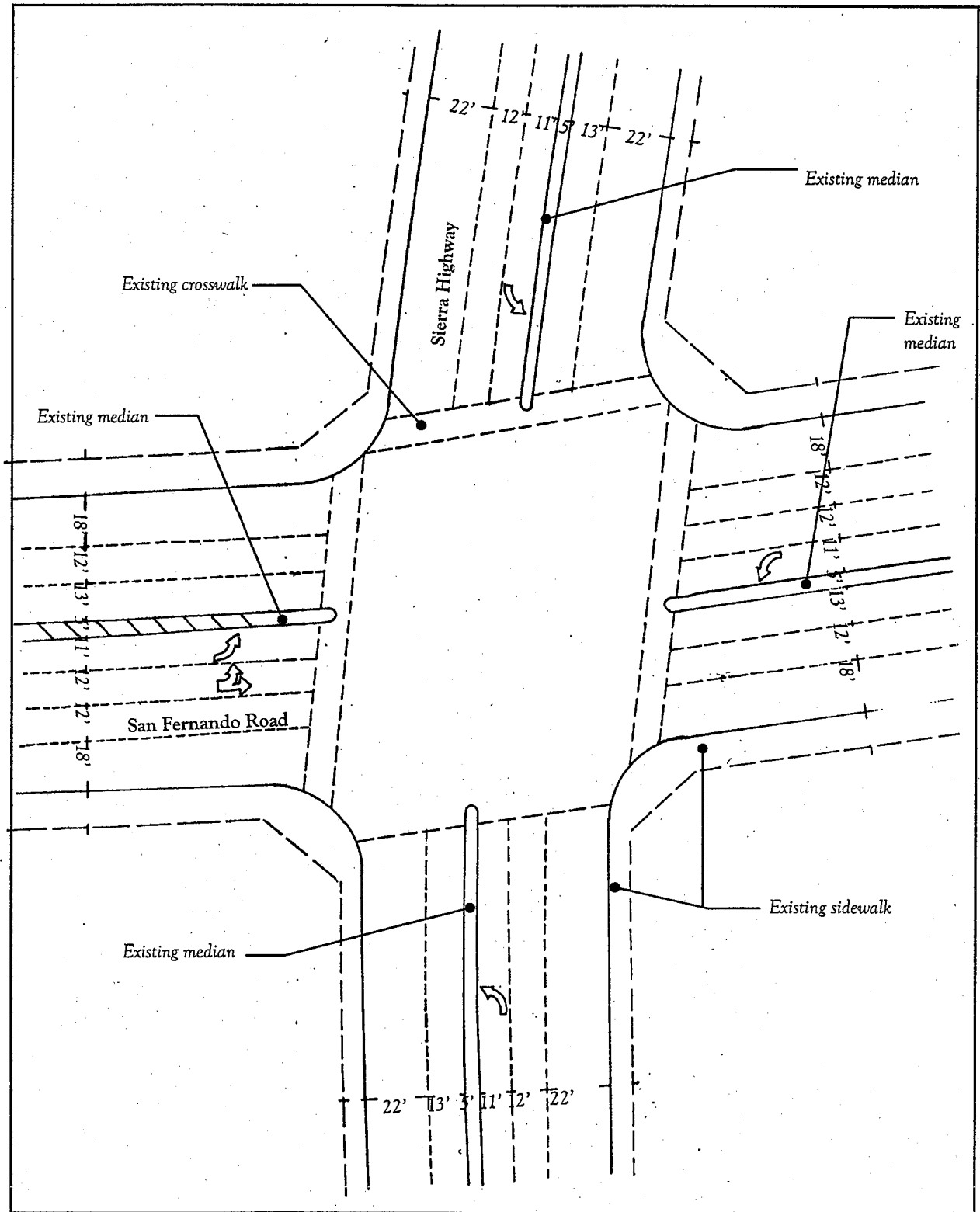
Additional vegetation located at key intersections will enhance the experience of the motorist and pedestrian. Landscaping, low walls and berms should be used to screen large expanses of paving (parking lots, service station lots, etc.) on private property. Medians should be widened to a minimum of six feet where feasible to allow for high branching trees to be planted. Trees with four-foot tree grates should be planted along the sidewalk near the intersection. The trees planted in the medians and in the parkways should correspond to those selected for a given community and primary corridor, respectively.

Lighting:

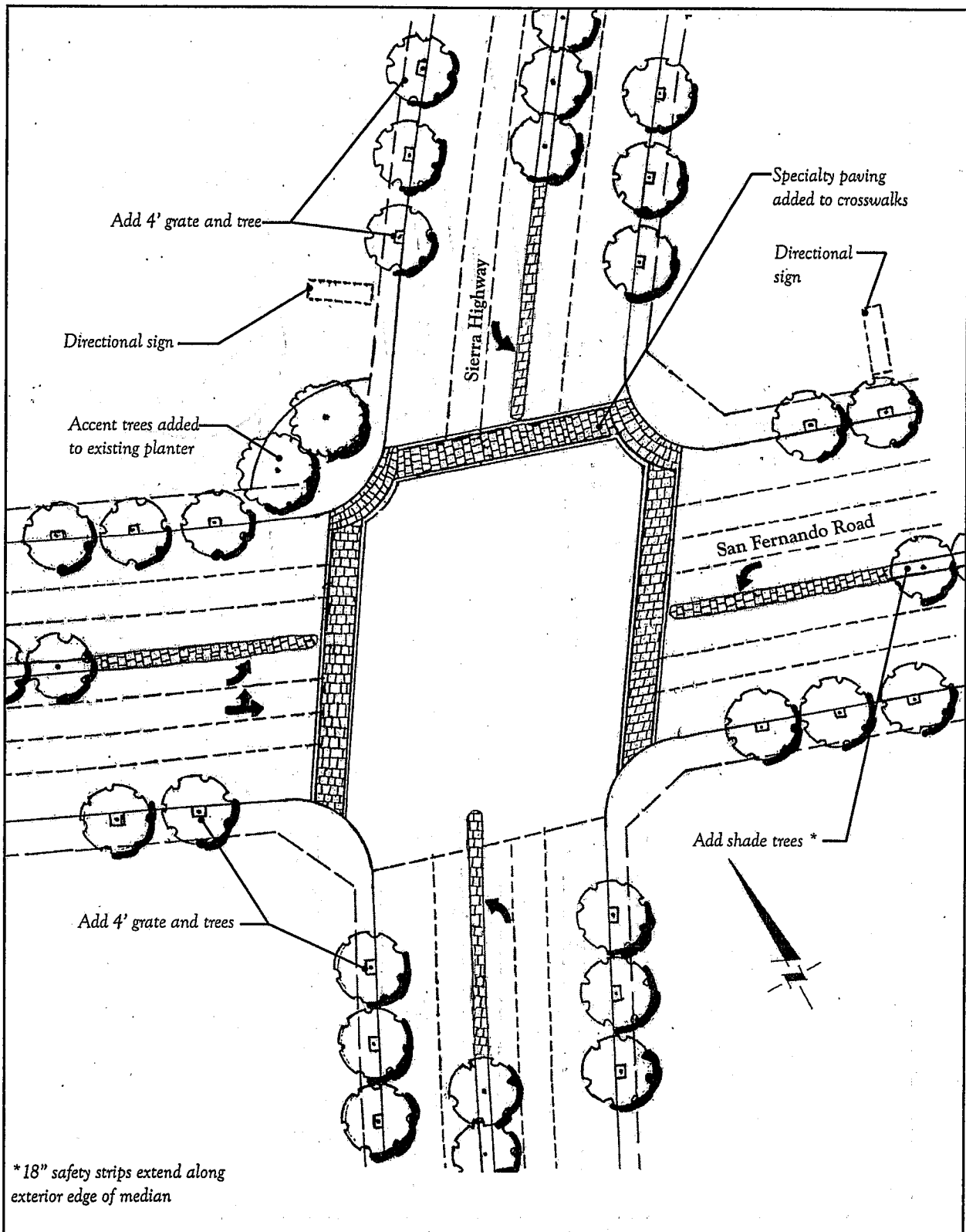
The implementation of a uniquely designed street light and signal poles at key intersections can enhance the experience of the pedestrian and motorist. A minor improvement involving the addition of an ornamental base and fixture to the existing pole can greatly improve its aesthetics. All specially designed street lights and signal poles shall be reviewed and approved by the appropriate agencies.



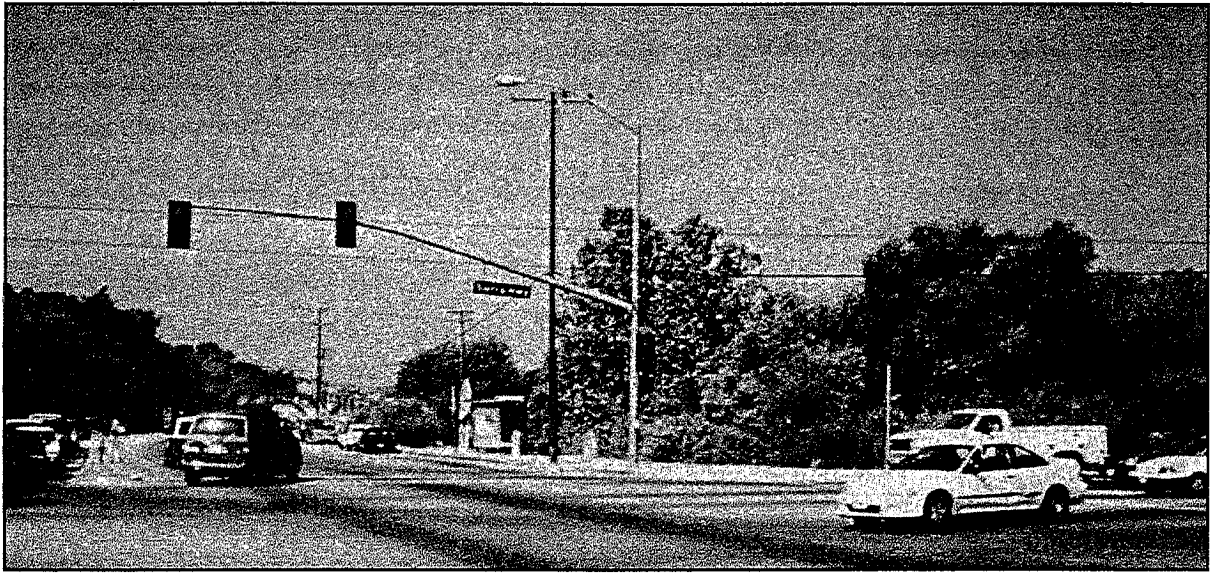
Example of Specially Designed Street Light and Signal Pole Enhancements



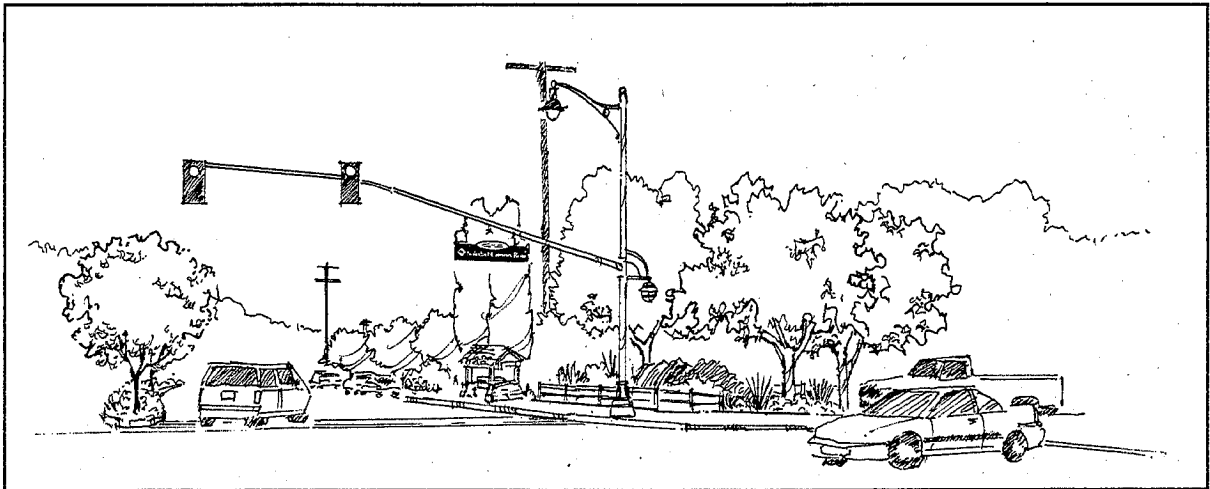
Before: Sierra Highway and San Fernando Road Intersection



After: Sierra Highway and San Fernando Road Intersection



Before: Sierra Highway and San Fernando Road Intersection



After Implementation of Beautification Improvements: Sierra Highway and San Fernando Road Intersection

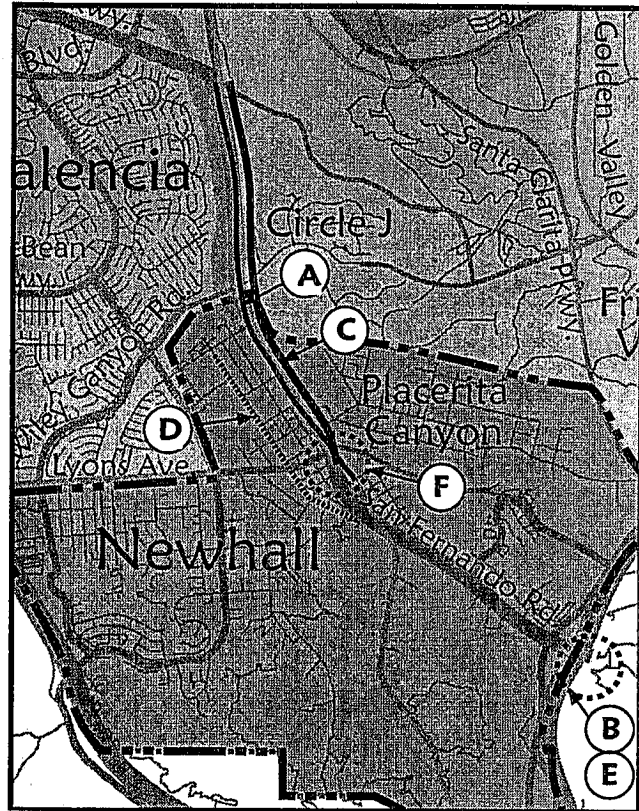
D. Other Beautification Ideas

This section addresses the other beautification needs identified by participants from various workshops and provides suggestions for additional ways in which a community might encourage beautification efforts.

1. Specific Beautification Needs

The following beautification needs are not directly addressed by the Beautification Master Plan, but are areas that community members have identified as issues needing improvement.

- A. Screening should be implemented along Railroad Avenue and San Fernando Road.
- B. The Highway 14 off-ramp at San Fernando Road should be improved.
- C. Additional medians and parkways should be planted along San Fernando Road north of Lyons Avenue.
- D. Medians and parkways should be added along Newhall Avenue.
- E. A sign identifying the Newhall exit should be placed on Highway 14.
- F. The portion of San Fernando Road between Newhall Avenue and Highway 14 could be renamed Newhall Avenue.



Newhall Beautification Needs

2. Private Property Treatment

To assist city beautification efforts within the street right-of-way, private property owners should strive to accomplish the following:

- Reduce sign clutter
- Landscape parking lots and building frontage
- Screen unsightly storage and trash areas
- Coordinate wall/fence treatment between properties
- Maintain vacant lots free of trash and weeds
- Seek to upgrade building facades consistent with design image
- Plant trees to add shade and soften urban look

3. Getting Involved In The Process

Community groups should make themselves aware of the City design review and approval process. Likewise, the City should notice community organizations of upcoming meetings and proposed projects. This established communication will help to ensure that projects are approved that residents and City staff are proud of. In addition, community groups might take on banner projects such as organizing and encouraging private property owners to improve their building facades or plant vines in front of unsightly walls and fences. The efforts of an organization can do a lot for a community.

E. Downtown Newhall Special Standards District

The purpose of this district is to strengthen the character of the downtown Newhall area with the intent to create a pedestrian-oriented "Old Town" district. All properties located within the Special Standards District are designated per the map provided in the Unified Development Code. This district gives special consideration to signs, lighting, street treatments and building facades and projects are subject to review and approval by the Newhall Redevelopment Committee.

SAUGUS

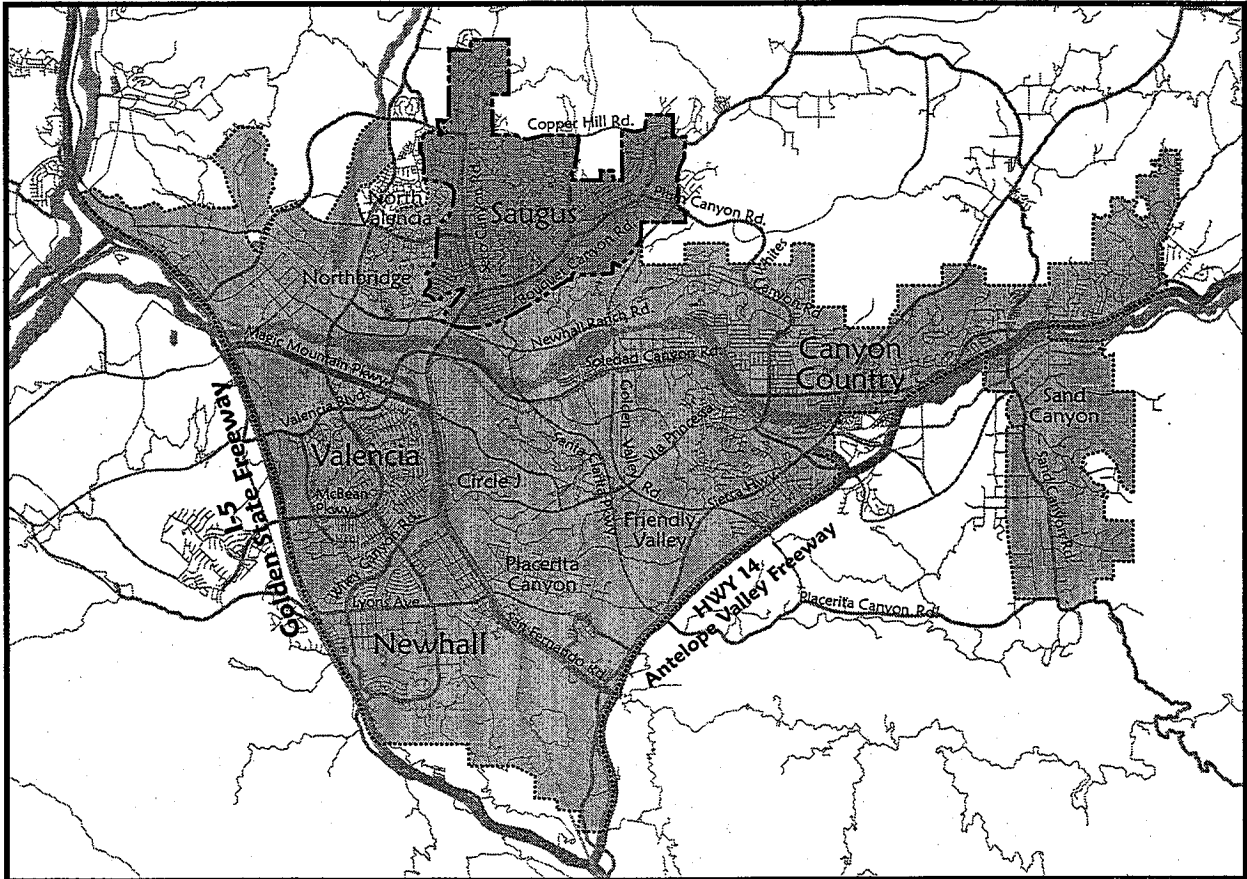
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IV. Saugus Community Design Guidelines

A. Introduction

The following beautification guidelines contain specific recommendations for the community of Saugus. Where there is overlap with the *Citywide Design Guidelines*, cross references are made. Though there is not a clear boundary for Saugus, the map below delineates the general area. These guidelines focus on areas within the public right-of-way in Saugus.



Community of Saugus Boundary

1. Community Identity

The community of Saugus was the second community to be developed in the Santa Clarita Valley. Originally, Saugus was developed as a rural community consisting of large residential lots with California ranch style homes and open spaces. Since that time, development has taken on different forms including smaller lots that back onto major thoroughfares and newer developments placed on hillsides and along slope faces and at ridgelines. However, much of northern Saugus is hilly and covered with natural vegetation. Residents of Saugus still think of themselves as a family-oriented, rural community. They value the surrounding canyons, abundance of cottonwoods and rustic and ranch character of the community.

Local residences used the following characteristics to describe Saugus during a public workshop. Items with an asterisk were identified as the best representative characteristics.

- Old mining community
- Ranching community
- Community of homes
- Family-oriented
- Hillside community
- Open space *
- Canyons
- Middle of the valley
- Small business
- Cottonwoods *
- Natural wildlife
- Gateway to natural forest *
- Large lots
- Rustic *
- Home to the road runner
- Sycamore and oak trees
- Soft natural green look – rather than clipped manicured look

2. Beautification Needs

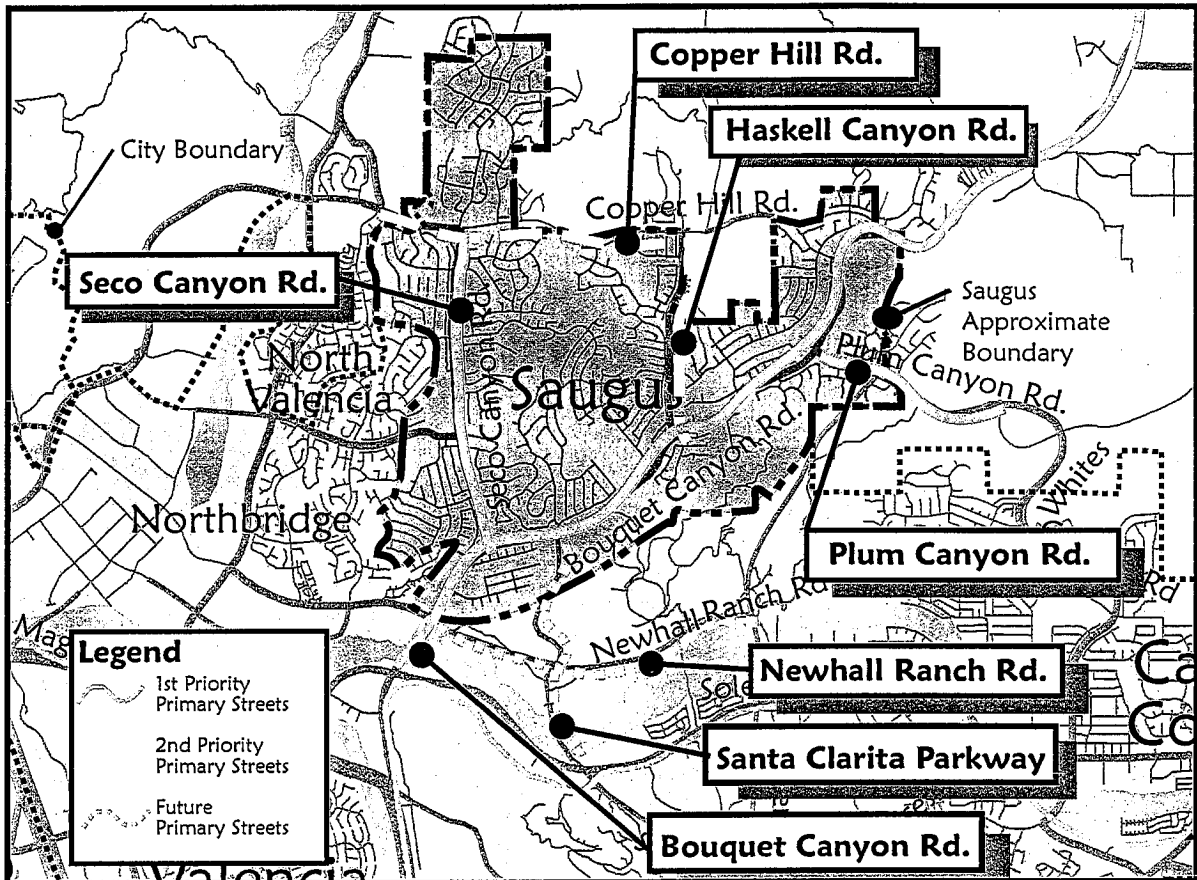
Recognizing the need for beautification in their community, Saugus has organized a number of volunteer groups that have committed many hours of hard work to help define community beautification goals and identify specific needs. These groups have contributed significantly to the contents of these guidelines.

The following list of beautification needs was generated at a public workshop conducted with members of the community, members of the Saugus volunteer organizations and Santa Clarita planning staff.

- Beautify major streets running through Saugus such as Bouquet Canyon Road.
- Extend median treatment to the perimeter of the community.
- Provide trash receptacles to minimize littering.
- Place electrical lines and other utilities underground.
- Encourage the use of Bouquet Canyon stone throughout Saugus.
- More trails should be created in Saugus.
- Key intersections should be designed consistently with crosswalks, landscaping, amenities and lighting.
- A unique logo that will help to unify and distinguish Saugus should be developed.
- Community entry signage should be designed with a logo image, colors and font that represents Saugus.
- Community identity should be strengthened, but not at the expense of the citywide identification.
- Bus stops should improved, sheltered and advertising benches should be replaced.
- Sign clutter should be reduced.

Based on this feedback from the community, beautification guidelines were drafted to address the following topics. The guidelines address a range of design improvements that can lead to a more cohesive and unified look for Saugus.

- | | | |
|----------------------|--------------------------|-------------------------------|
| 1) Primary corridors | 4) Street edge treatment | 7) Main intersections |
| 2) Signs | 5) Fences | 8) Other beautification ideas |
| 3) Medians | 6) Community entry | |



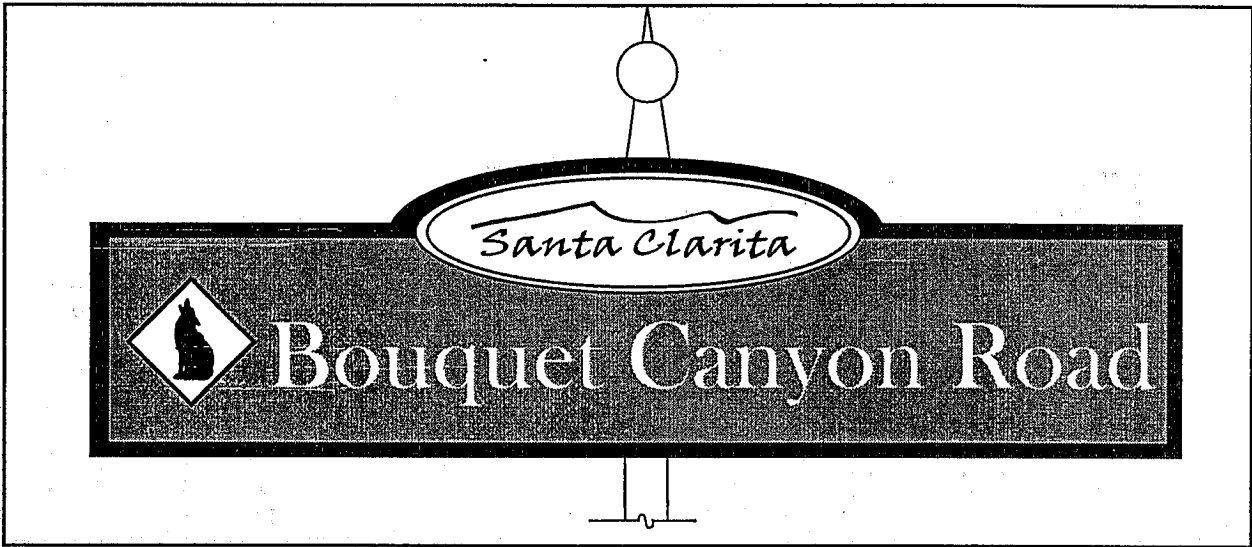
Saugus Primary Streets

B. Treatment of Primary Corridors

The primary corridors that extend through a given community act as the most visible and consequently the strongest unifying element within the community. Each community within the City of Santa Clarita should be clearly identifiable with distinct primary corridors that help unify it. The implementation of a community logo on each street sign, a consistent median and corridor edge treatment, a unique fence type and landscaping improvements bring together the image of a cohesive community. One goal of the Beautification Master Plan is to set forth long-range guidelines for improvements, enhancements and beautification of such corridors so that implementation occurs in a direction that creates a clear and easily recognizable individual community. The following streets have been designated as primary corridors within Saugus.

- First Priority Primary Corridors: Seco Canyon Road, Bouquet Canyon Road and Plum Canyon Road
- Second Priority Primary Corridors: Copper Hill Road and Haskell Canyon Road
- Future Primary Corridors: Newhall Ranch Road and Santa Clarita Parkway

1. Signs and Logo



Example of a Street Sign Designed to Incorporate a Saugus Logo

Street Sign:

The most prevalent and obvious component of the primary corridor elements is the street sign treatment. It is here that a clear and distinct demarcation occurs for each individual community. Every street sign within the community of Saugus should contain a community logo. This logo provides a recognizable sense of repetition that clearly enables a motorist, bicyclist or pedestrian to know where they are and if they are still within the community limits. The logo will be placed within a diamond on the left side of the sign and each diamond will be a specific color for each community. This simple and quickly recognizable feature will provide a highly visible statement as to the community that an individual is in. Refer to the *Citywide Signage Program* for further information.

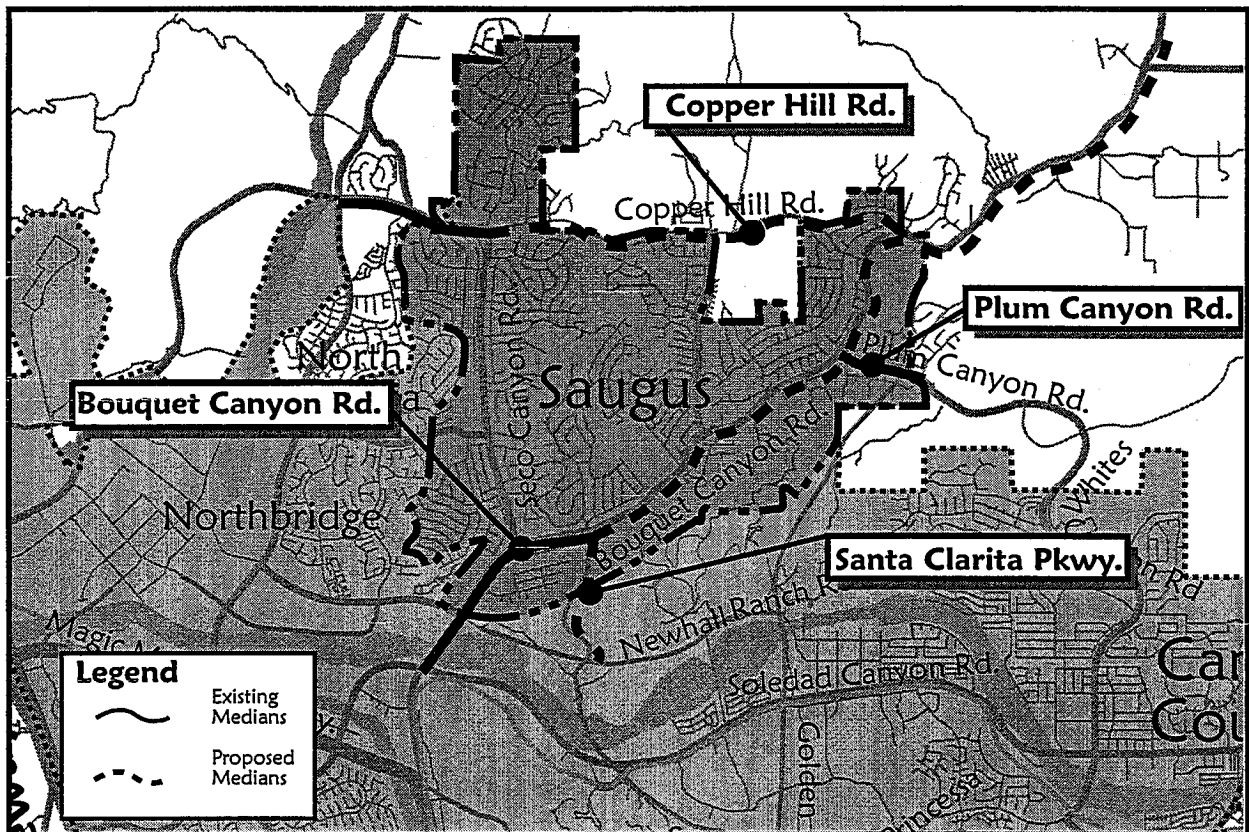
Saugus Community Logo:

Through the interview and public outreach process, participants generated a list of ideas for a Saugus icon. The icon should be unique to Saugus and not something that could represent any number of areas in the Santa Clarita Valley. It should be a logo or symbol that is timeless, avoiding trendy images, colors or fonts. The logo will be designed at the time of implementation, however, some possibilities include:

- Coyote with ridges
- Canyon / creek
- Angeles National Forest
- Bouquet Canyon stone
- Oak trees and Sycamores



Example of Possible Saugus Community Logo



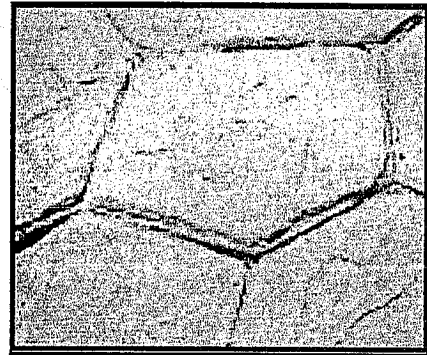
Saugus Medians Map

2. Median Treatment

One intent of the Beautification Master Plan is to build on and extend Santa Clarita’s existing network of roadway medians into one, cohesive citywide plan that identifies each community. Each community will have a planting theme to promote the area. The plant palette selection that will be used in the beautification and implementation of the medians will continue to enhance the adjacent landscape. The materials will be consistent within a given community and act as a clear indicator of the difference between communities. New raised and planted medians should be considered a long-term objective for community beautification. Along with providing aesthetic appeal and consistency, the proposed medians will also control vehicle circulation, movements and serve to calm traffic. The medians will also provide a place for roadway approach street signs and directional signage at key intersections as part of the citywide signage program discussed in the *Citywide Design Guidelines*. Within Saugus and vicinity, the selected median treatment will be applied to the following roads at the locations indicated on the figure above.

- Copper Hill Road - from the existing median east to Bouquet Canyon Road
- Bouquet Canyon Road - from the existing median to the north
- Santa Clarita Parkway - from Bouquet Canyon Road south to Newhall Ranch Road
- Plum Canyon Road - existing median

Currently, median maintenance is funded by either City General funds or privately funded Landscape Maintenance Districts. These different funding sources provide opportunities for different median design concepts. Both designs incorporate a similar plant palette that will help to enhance and unify the community. A drought tolerant design offers a lower maintenance solution with plants that have an ability to thrive in tough conditions and require less water. A wide variety of plants have been selected that can add beauty and diversity to any landscape. A combination of crescent shaped landscaped pockets and stamped concrete paving should continue to be used in areas that are funded by the City General fund.



Bouquet Canyon Stone Formed Stamped Concrete Safety Strip

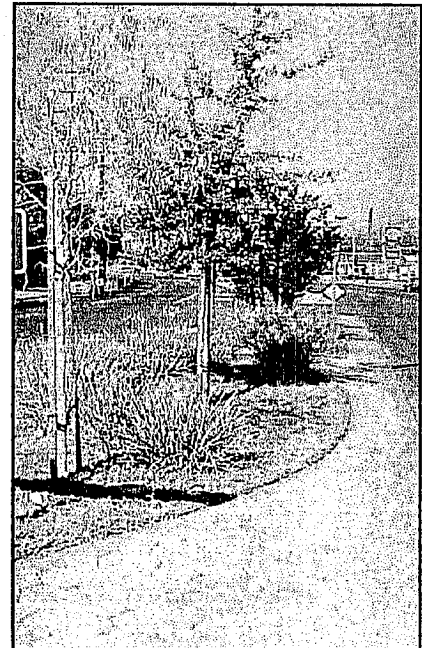
The median treatment Saugus utilizes is a drought tolerant landscaping design that incorporates a drip irrigation system. The landscaping is surrounded with an 18-inch safety curb that is a stamped concrete shaped and colored like Bouquet Canyon stone.

The following landscaping palette has been created using and enhancing existing plant palettes used within the area. Future planting should utilize this list or plants that are similar and compliment the desired theme.

Median Landscaping

Trees Types which are being used

Botanical Name	Common Name
<i>Geijera parviflora</i>	Australian Willow
<i>Prunus cerasifera</i> 'Atropurpurea'	Purple Leaf Plum
<i>Pyrus species</i>	Ornamental Pear
<i>Quercus agrifolia</i>	Coast Live Oak



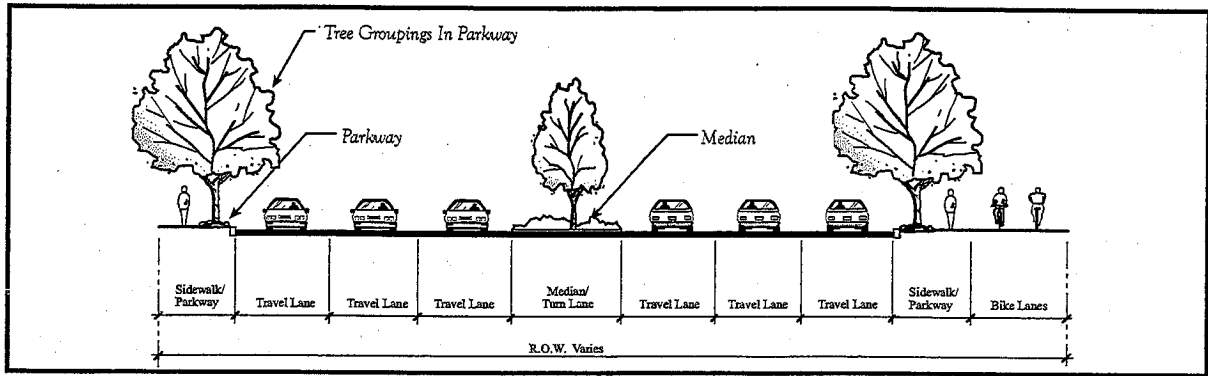
Saugus Median Treatment

Shrubs which are being used

Botanical Name	Common Name
<i>Dietes vegeta</i>	Fortnight Lily
<i>Rhaphiolepis umbellata</i> 'Minor'	-----
<i>Rhaphiolepis indica</i> 'Clara'	India Hawthorn

Ground Cover which are being used

Botanical Name	Common Name
<i>Myoporum parvifolium</i> 'Putah Creek'	-----



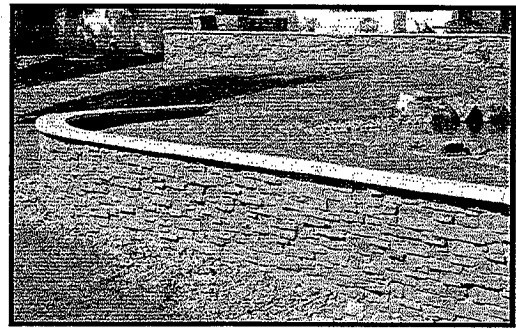
Example of Roadway Section

3. Street Edge Treatment

The sides of the primary corridors are just as important as the median treatment. Well landscaped parkways provide pleasant driving and walking environments while creating a sense of place. A planting palette for such parkways is included in the *Citywide Landscaping Palettes* section of this document.

Fences / Walls:

Fences or low walls should be used to unify the City of Santa Clarita. They provide accent and help define a space and can be used to buffer parking lots adjacent to the roadway. In addition, fences used at vacant lots can “bridge” large void spaces and provide unity along the street scene. Each community will incorporate its own fencing /wall type at intersections and along primary corridors where appropriate. Stone walls, that are in combination with landscaping, best characterize the community of Saugus. At the community workshops, Bouquet Canyon Stone walls were the preferred wall / fence type. In cases where this treatment is not affordable, a man-made stone that replicates Bouquet Canyon Stone may be used. Another cost saving treatment would be to use the stone walls in combination with a post and rail fence. Through the interview and public outreach process, participants generated the following list of ideas for a Saugus fence/wall type.

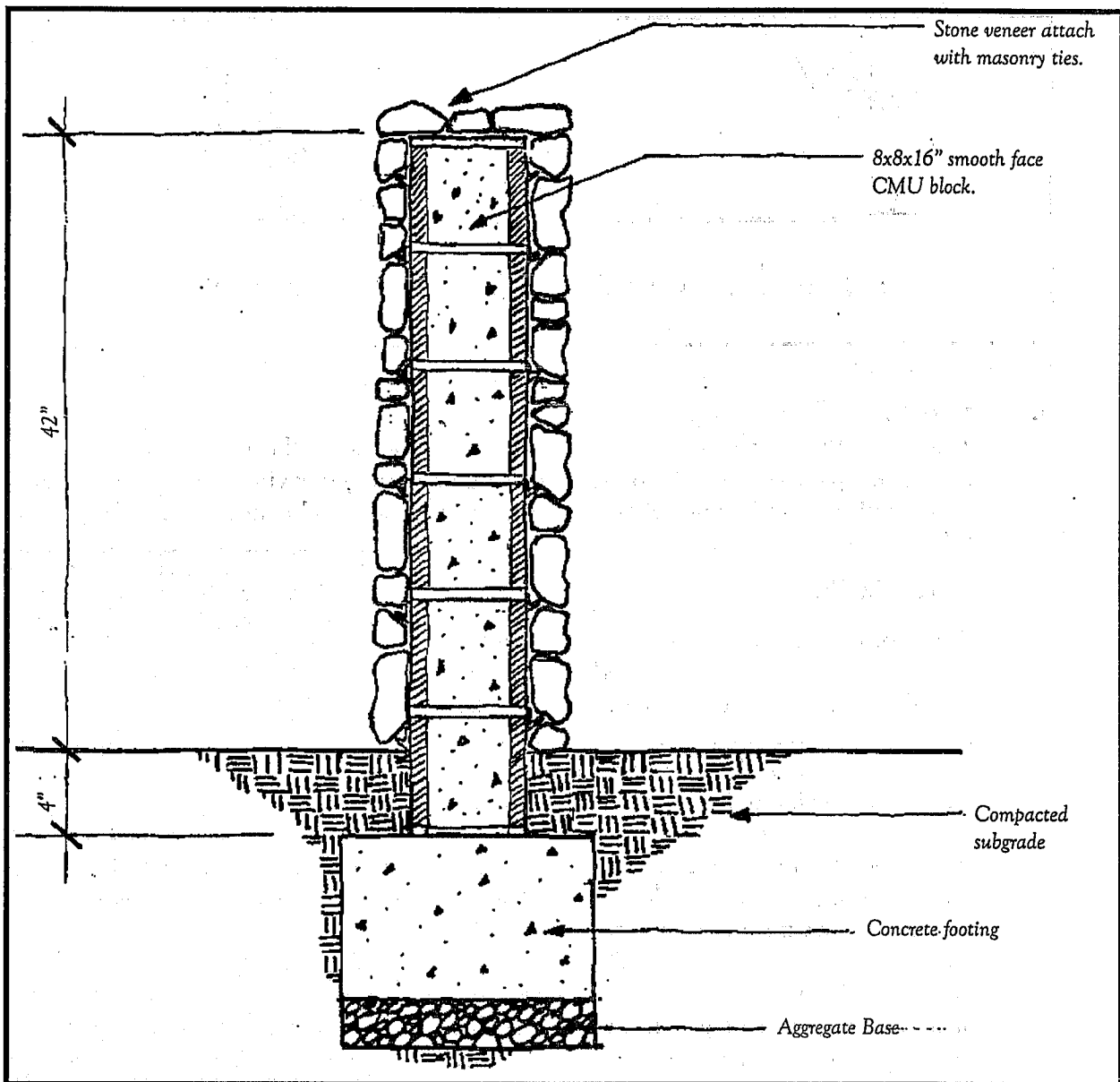


Example of Bouquet Canyon Stone Wall



Example of Bouquet Canyon Stone and Lodge Pole Fence

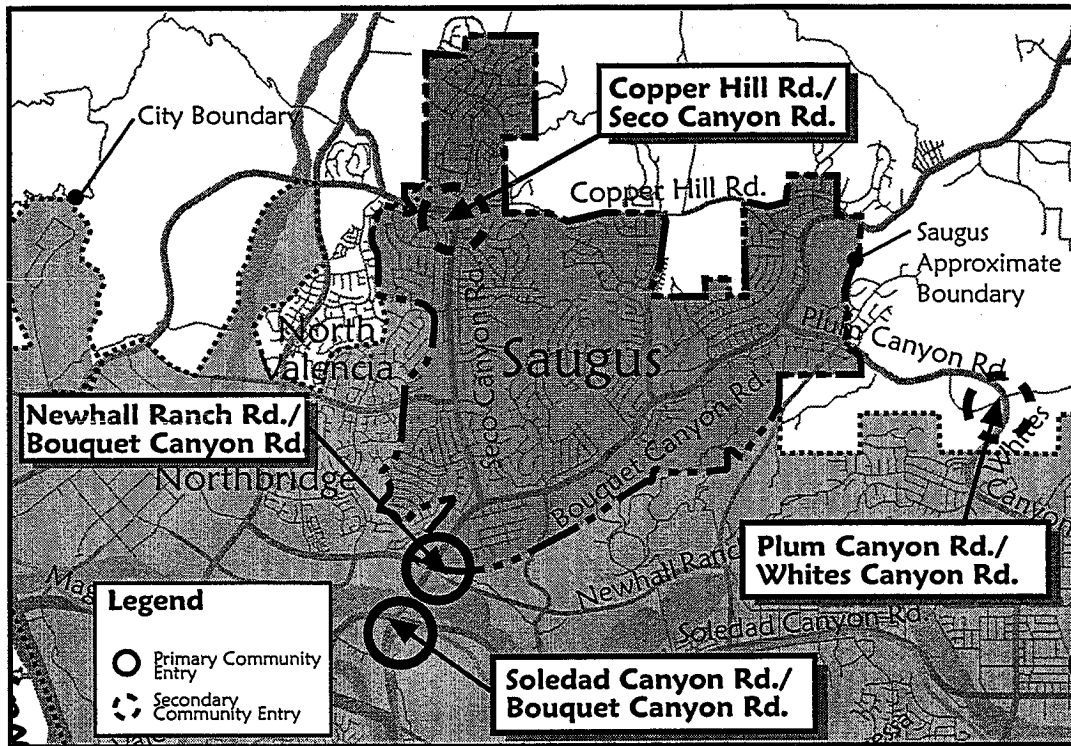
- Use Bouquet Canyon Stone as much as possible.
- Rock walls are preferred over fences.
- Stone walls need to be landscaped to avoid a desert feel.
- Walls and landscaping should give a finished natural appearance avoiding a “rustic” look.



Example of Saugus Wall

The rock wall is composed of a 8"x8"x16" smooth face block wall core with rebar as required by local code for structural stability. A 520-C-2500 P.C. concrete should be used to fill all voids. The stone veneer should be Bouquet Canyon ledger stone attached with masonry ties. Concrete footings should be used for structural stability and placed over an aggregate base to allow water to drain through.

Alternative construction method: Economical, long lasting precast concrete panels, posts and caps can be used as an alternative. This precast wall system should be of integrally colored concrete and the system should replicate a natural Bouquet Canyon stone wall. Both sides of the wall should have an authentic textured appearance on both sides.



Saugus Entrances Map

C. Community Entrances and Intersections

1. Community Entrance Treatment and Locations

The entrance to each individual community should have a unique and easily identifiable “scene” that includes specific landscaping, a fence / wall and a unique community-oriented sign. The entrances should give visitors and residents a feeling of what to expect within the community and be welcoming without being overstated. The following locations have been selected for community entrance treatments.

Primary Community Entries:

- Soledad Canyon Road and Bouquet Canyon Road
- Newhall Ranch Road and Bouquet Canyon Road

Secondary Community Entries:

- Copper Hill Road and Seco Canyon Road
- Plum Canyon Road and Whites Canyon Road

2. Community Entrance Design

Community Entrance Character:

The community entrance is one of the best opportunities to reflect individuality. The character and materials used should be a unique representation of the community. The following materials and design styles were identified during a public workshop to reflect the character of Saugus.

- Soft, natural, green vegetation
- Bouquet Canyon stone is important and should be used in entry signage
- Cottonwood and oak trees



Proposed Saugus Entry Scene - Cottonwood and oak trees, Bouquet Canyon Stone wall, boulders, special plant palette, and a community-oriented sign with community logo and City name.

Community Entry Signs:

In the *Citywide Design Guidelines* section, "Entry Monuments" will help meet the goal to unify the City as a whole while the "Community Entry Signs" will help meet the other half of the goal, to embrace the different characters of individual communities. The Saugus entry sign reflects the unique natural setting and materials of the area. Community entry signs shall have the following characteristics:

- Community entry signs should be located at each of the previously mentioned community entrance locations and should be coordinated with Caltrans or private landowners as appropriate. Specific locations should be determined based on visibility, right-of-way availability, sight distance criteria and minimum square footage requirements.
- Community entry signs should be lighted and landscaped.
- Community entry signs should be an integral part of the gateway scene which includes an entry sign, a fence and plants and trees.
- Community entry signs should be well proportioned and accented with landscaping. The scale of the signs should be easily legible from the road. Planting at maturity must not obscure safe vehicular sight lines.
- Community entry signs should incorporate complementary colors, materials and

lettering fonts. More than one material should be used in the construction of the sign and should reflect the character of the community. The sign should be constructed for long-term durability and easy maintenance.

- All community entry signs should use the same materials, color scheme and graphic theme in order to make it very clear to visitors and residents alike that they are placed as an effort to unify Saugus.
- The community logo should appear on each sign and a community-specific font should be replicated exactly on each Saugus entry sign.
- “City of Santa Clarita” should appear on each community entry sign. The same font should be used for the citywide entry monuments.

The following landscaping palette has been created using and enhancing existing plant palettes applied throughout the Saugus area. Future planting should utilize this list or plants that are similar and compliment the desired theme.

Community Entrance Landscaping:

Trees

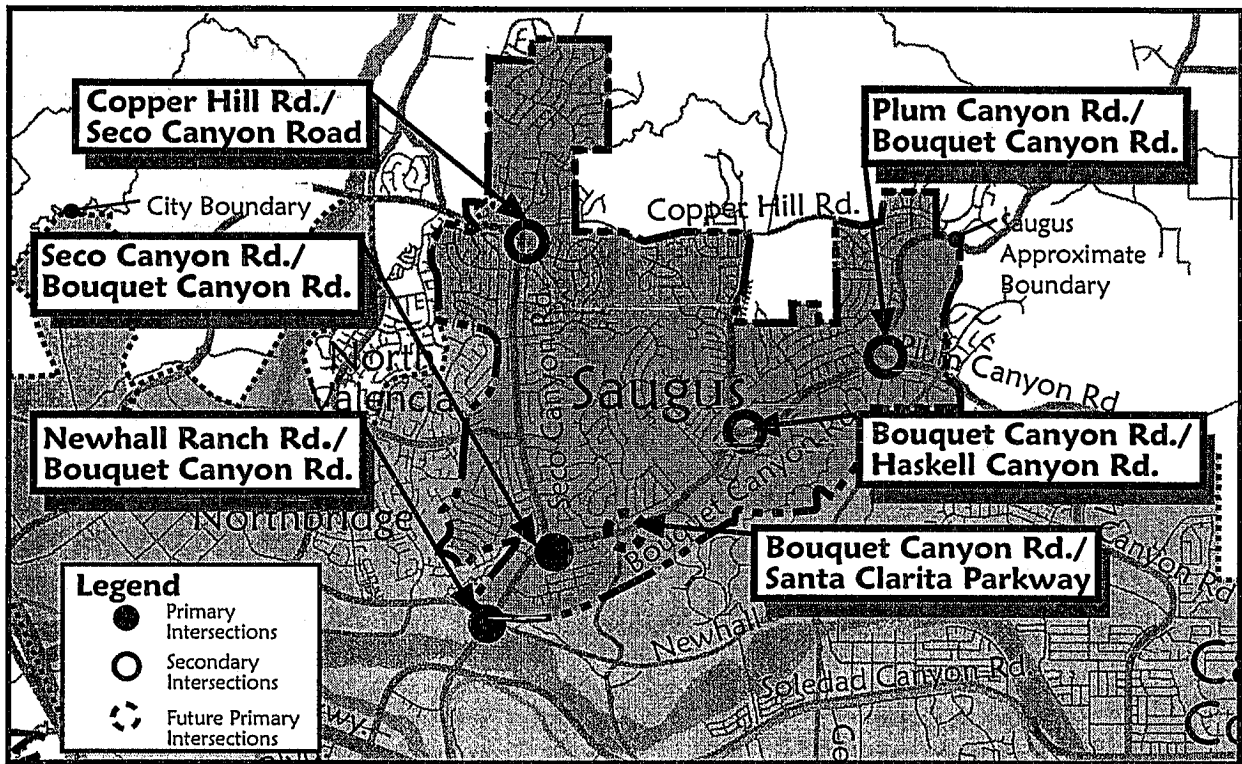
Botanical Name	Common Name
<i>Populus fremontii</i>	Western Cottonwood
<i>Quercus agrifolia</i>	Coast Live Oak

Shrubs

Botanical Name	Common Name
<i>Heuchera sanguinea</i>	Coral Bells
<i>Phormium tenax</i> ‘Maori Chief’	New Zealand Flax
<i>Rhaphiolepis umbellata</i> ‘Minor’	-----
<i>Romneya coulteri</i>	Matilija Poppy

Ground Cover

Botanical Name	Common Name
<i>Arctostaphylos</i> ‘Emerald Carpet’	Manzanita
<i>Achillea tomentosa</i>	Woolly Yarrow
<i>Convolvulus mauritanicus</i>	Ground Morning Glory
<i>Gazania</i> ‘Mitsuwa’	Gazania
<i>Myoporum p.</i> ‘Putah Creek’	-----



Key Intersections Map

3. Key Intersection Treatment and Locations

There are several intersections within the community of Saugus that stand out as opportunities to beautify and claim as unique places for Saugus. These intersections are large with multiple lane thoroughfares requiring motorists and pedestrians alike to wait for several minutes and soak in the surrounding environment. Pedestrians oftentimes have to cross over several lanes of traffic in order to travel from place to place. It is the intent of these guidelines to beautify these intersections to the extent possible and create a more pedestrian-friendly environment. The following key intersections have been identified as areas that need priority improvement.

Primary Intersections

- Bouquet Canyon Road and Soledad Canyon Road
- Newhall Ranch Road and Bouquet Canyon Road
- Seco Canyon Road and Bouquet Canyon Road

Secondary Intersections

- Copper Hill Road and Seco Canyon Road
- Plum Canyon Road and Bouquet Canyon Road
- Bouquet Canyon Road and Haskell Canyon Road

Future Primary Intersections

- Bouquet Canyon Road and Santa Clarita Parkway

Beautification of the intersections should include:

- Accent paving
- Directional signage / reduction of visual clutter
- Landscaping and street trees
- Lighting and signal pole treatment
- Underground utility poles and wires

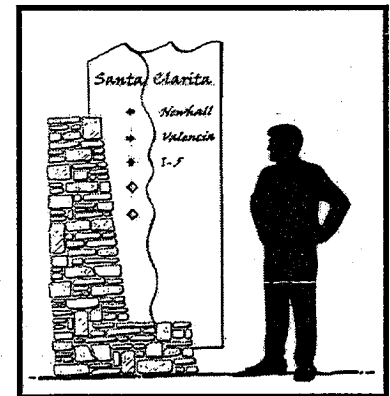
The Seco Canyon Road and Bouquet Canyon Road intersection illustrated on the following pages depicts the advantages of implementing some of these beautification concepts.

Accent Paving:

The use of accent paving provides an opportunity to both dress up the intersection and incorporate traffic calming measures for pedestrian and bicyclist safety. The change in texture is a visual and intuitive signal to both the motorist and pedestrian of the appropriate boundaries for each. Accent paving contributes greatly to the overall visual appeal of the intersection and is typically located in one or more of the following areas: at corners; on sidewalks; at pedestrian crossings; and in the middle of intersections. At key intersections, the pedestrian crosswalks should be treated with a durable colored concrete pattern so as to visually cue the oncoming motorist to slow down because of possible pedestrian traffic. Special paving also enhances the pedestrian experience, both in visual appeal and safety. The texture should be smooth enough so that an older person, cyclist and persons with disabilities are not deterred. In addition, a surface that limits noise should be utilized. Colored concrete stamped in a Bouquet Canyon Stone pattern should be used in crosswalks at key intersections.

Signage:

The directional sign described in the *Citywide Signage Program* section of the *Citywide Design Guidelines* should be placed at key intersections in order to help motorists determine the direction of the adjacent communities. It is important that existing directional signage be removed to reduce the amount of visual clutter. In addition to directional signage, the removal or replacement of commercial/business signage and billboard/advertisement signs should be given first priority at key intersections.



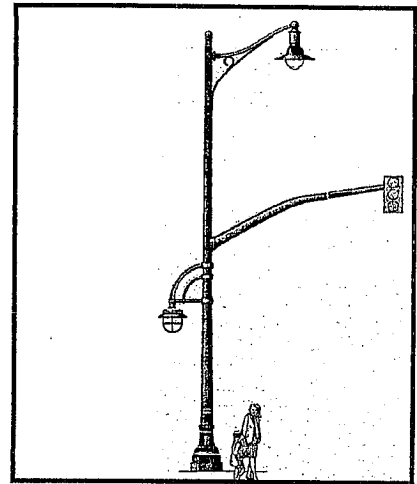
Example of Directional Signage

Landscaping Treatments:

Additional vegetation located at key intersections will enhance the experience of the motorist and pedestrian. Landscaping, low walls and berms should be used to screen large expanses of paving (parking lots, service station lots, etc.) on private property. Medians that are wider than 3'-0" should be planted. A typical median width is 14'-0" that will allow for high branching trees to be planted. Trees with four-foot tree grates should be planted along the sidewalk near the intersection. The trees planted in the medians and in the parkways should correspond to those selected for a given community and primary corridor, respectively.

Lighting:

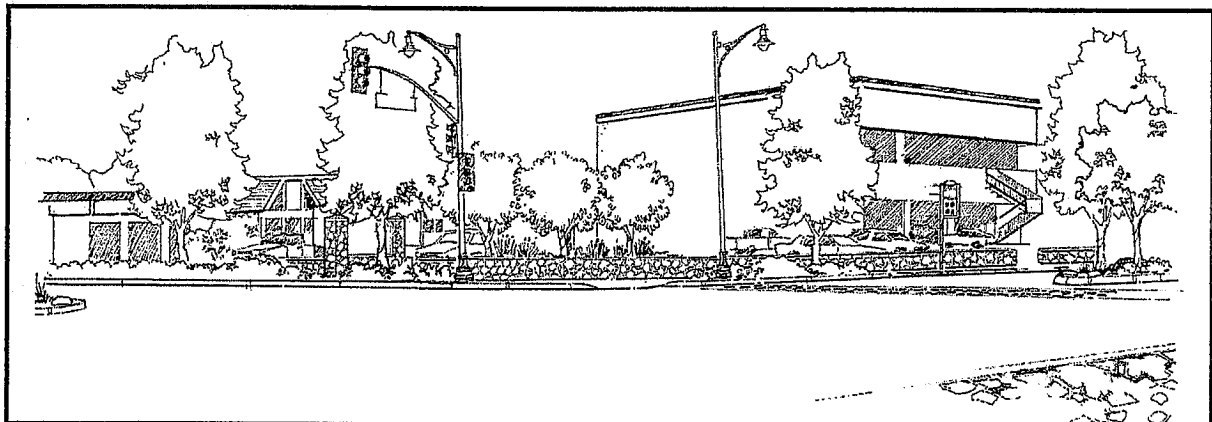
The implementation of a uniquely designed street light and signal poles at key intersection can enhance the experience of the pedestrian and motorist. A minor improvement involving the addition of an ornamental base and fixture to the existing pole can greatly improve its aesthetic appeal. All specially designed street lights and signal poles shall be reviewed and approved by appropriate agencies.



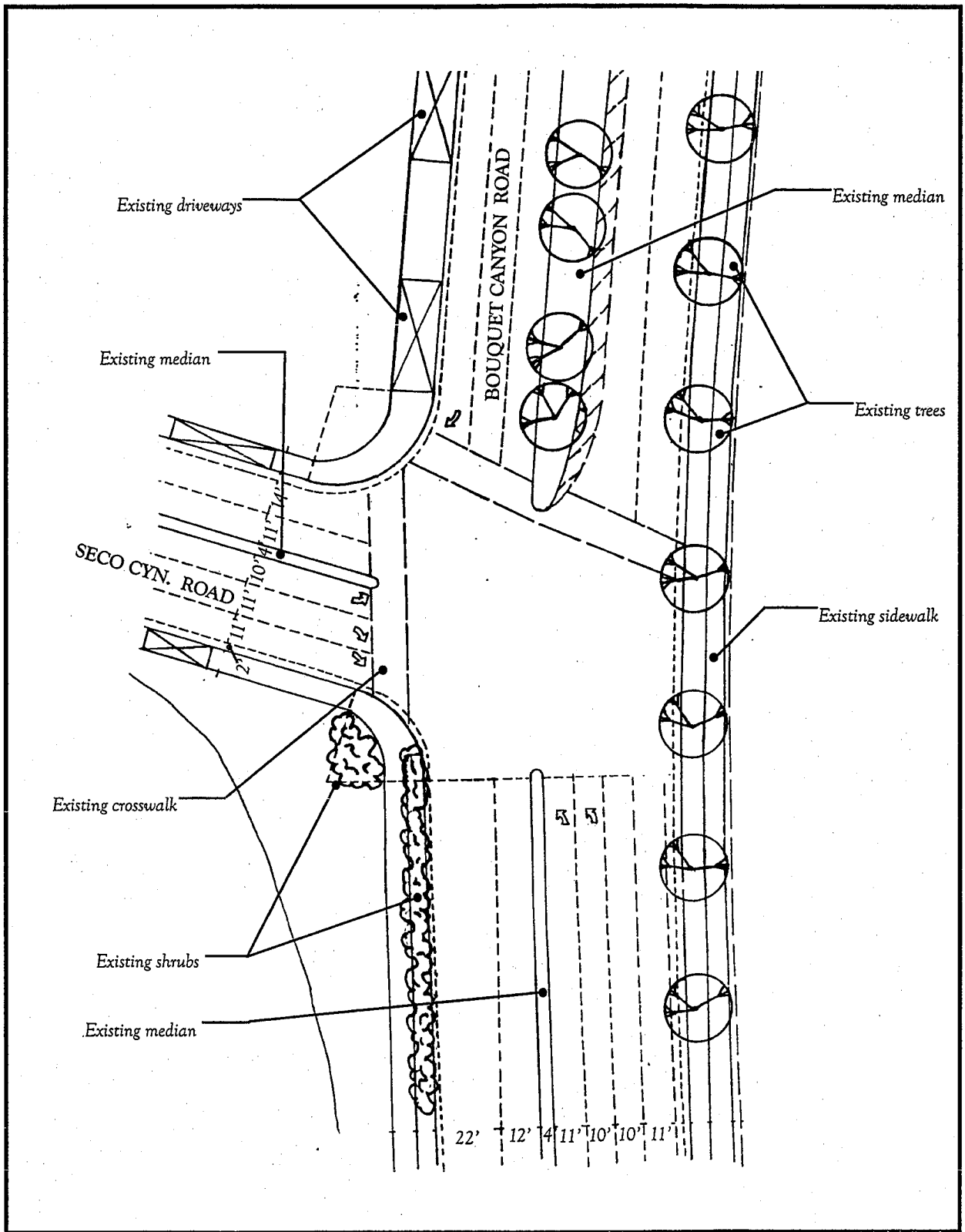
Example of Specially Designed Street Light and Signal Pole Enhancements



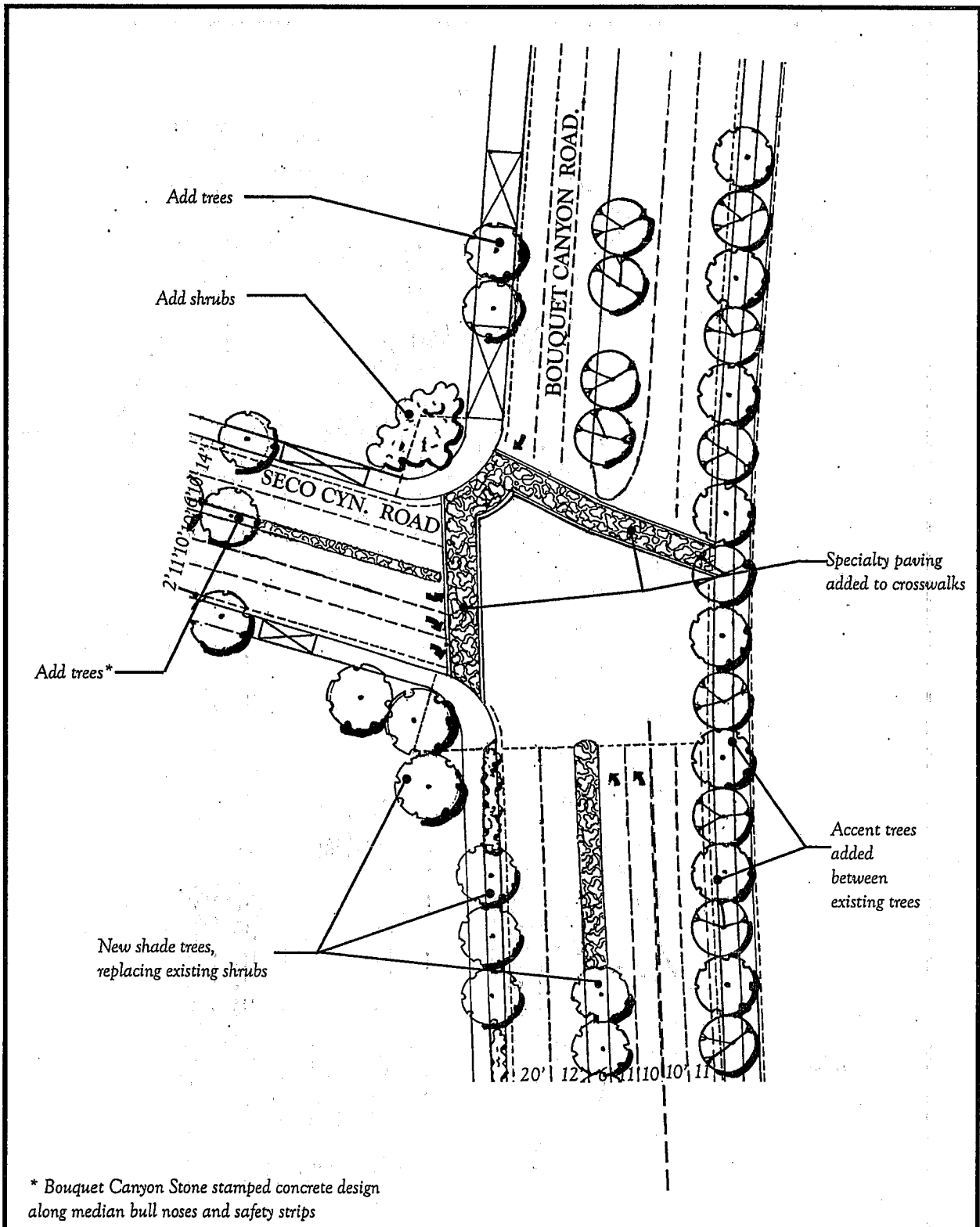
Before: Seco Canyon Road and Bouquet Canyon Road Intersection



After: Seco Canyon Road and Bouquet Canyon Road Intersection



Before: Seco Canyon Road and Bouquet Canyon Road Intersection



After: Seco Canyon Road and Bouquet Canyon Road Intersection

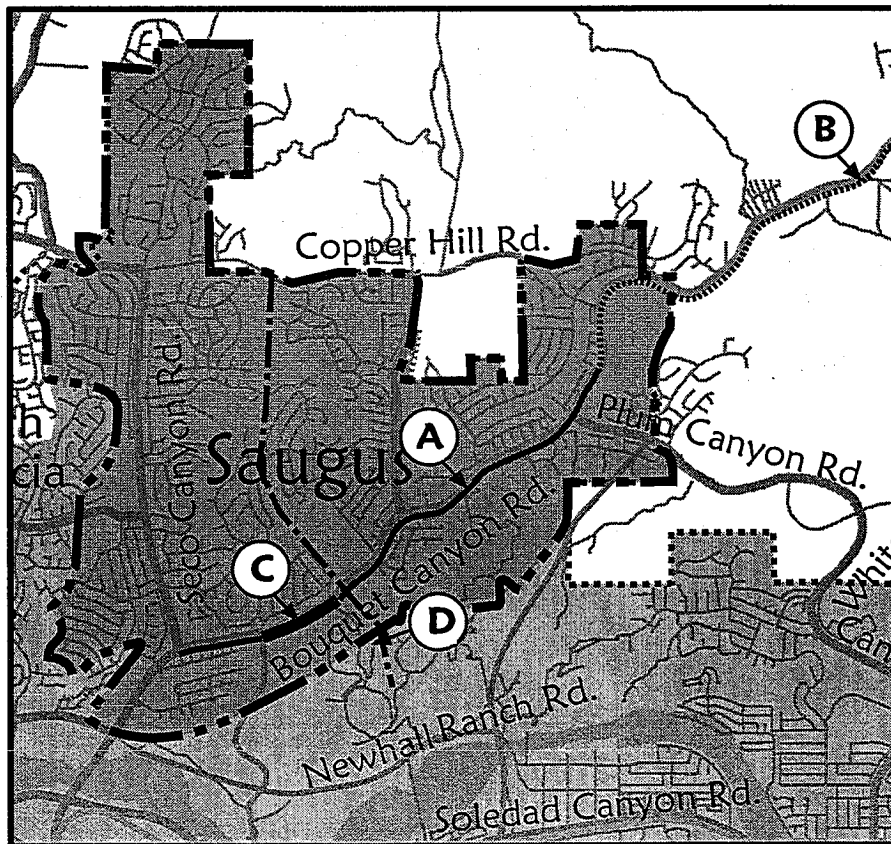
D. Other Beautification Ideas

This section addresses the beautification needs identified by participants from various workshops and provides suggestions for additional ways in which a community might encourage beautification efforts.

1. Specific Beautification Needs

The following beautification needs are not directly addressed by the Beautification Master Plan, but are areas that community members have identified as issues needing improvement.

- A. Central Park / Bouquet Canyon corridor
 - Remove privately-owned chain link fences and replace with solid block walls
 - Incorporate Bouquet Canyon Stone pilasters with planting
- B. North Bouquet Canyon Road
 - Beautify medians on Bouquet Canyon Road from Saugus High School north past Plum Canyon Road to Scallion Drive
- C. Alamagordo Road and Bouquet Canyon Road
 - Private slope bank needs planting, possible City landscape easement
- D. Screen Los Angeles Aqueduct pipe



Saugus Beautification Needs

2. Private Property Treatment

To assist city beautification efforts within the street right-of-way, private property owners should strive to accomplish the following:

- Reduce sign clutter
- Landscape parking lots and building frontage
- Screen unsightly storage and trash areas
- Coordinate wall/fence treatment between properties
- Maintain vacant lots free of trash and weeds
- Seek to upgrade building facades consistent with design image
- Plant trees to add shade and soften urban look
- Use fencing and landscaping along roadway at vacant parcels

3. Getting Involved In The Process

Community groups should make themselves aware of the City design review and approval process. Likewise, the City should notice a member of the organization of upcoming meetings and proposed projects. This established communication will help to ensure that projects are approved that residents and City staff are proud of. In addition, community groups might take on banner projects such as organizing and encouraging private property owners to improve their building facades or plant vines in front of unsightly walls and fences.

VALENCIA

Valencia

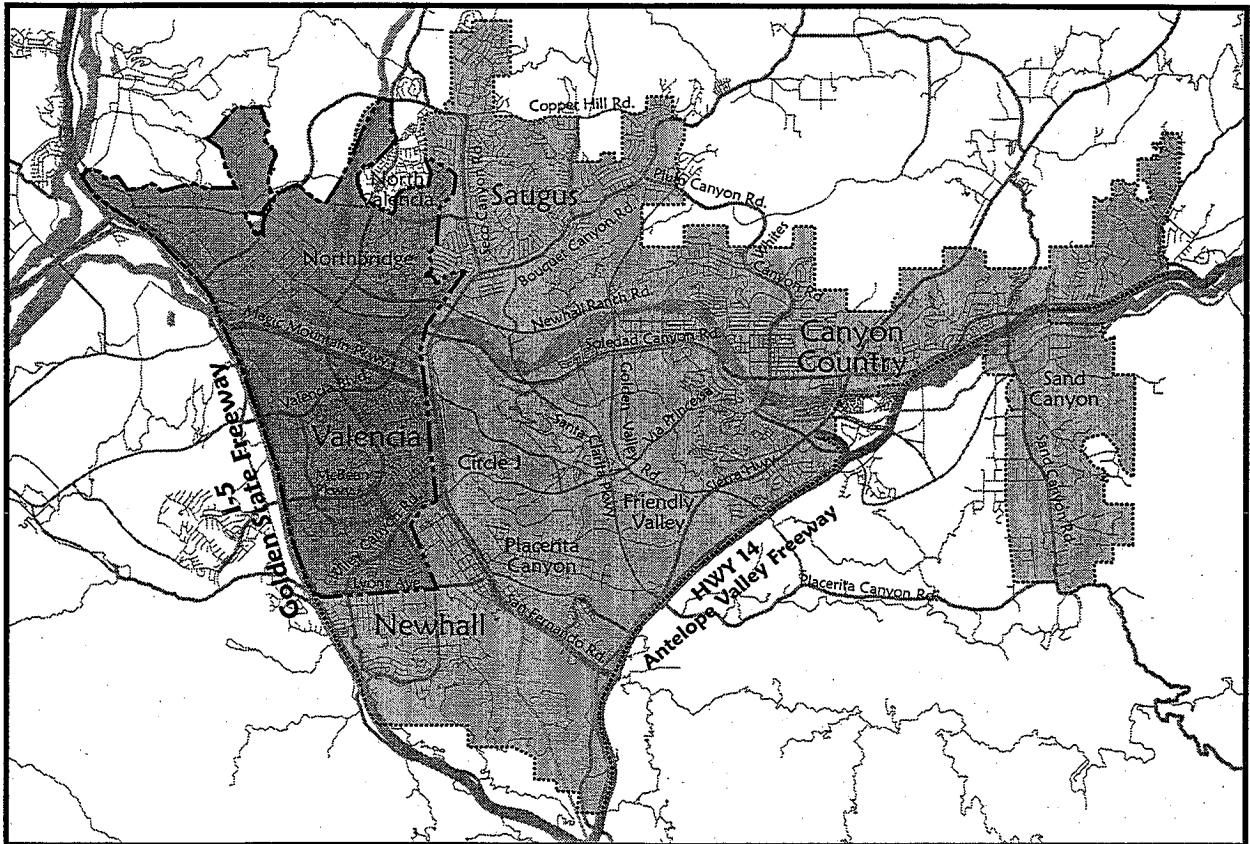
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V. Valencia Community Design Guidelines

A. Introduction

The following beautification guidelines contain specific recommendations for the community of Valencia. Where there is overlap with the *Citywide Design Guidelines*, cross references are made. Though there is not a clear boundary for Valencia, the map below delineates the general area. These guidelines focus on areas within the public right-of-way in Valencia.



Community of Valencia Boundary

1. Community Identity

Valencia was started as a master plan community created by planner Victor Gruen in 1965. The community includes everything from places to live, work and shop to schools, a hospital and entertainment and recreation facilities. There are over 21 miles of pedestrian walkways known as paseos that run through Valencia and make it easy to bike or walk to any number of different destinations. Much of Valencia's public right-of-way, including the medians, have been enhanced as part of new development and maintained by privately-funded landscape maintenance districts (LMD). Because of these enhancements and the LMD funds, Valencia has been landscaped in a more lush and manicured fashion than the surrounding communities. Well maintained streetscapes with green grassy medians, bright and colorful flowers at entrances, white three-rail fences and tree-lined streets are abundant throughout Valencia.

Though probably the most urban community in Santa Clarita, Valencia still has a touch of rural character. It is the combination of lush landscaping, urban architecture and sophistication in a rural setting that defines Valencia's character. The following characteristics were used to describe Valencia during a public workshop.

- Family
- High quality of life
- Cleanliness
- Paseo system
- Safe and secure
- Lush landscaping
- Pedestrian friendly
- Well lit
- Tight knit community
- Stability of real estate values
- Community pride
- Pristine marketing corridor

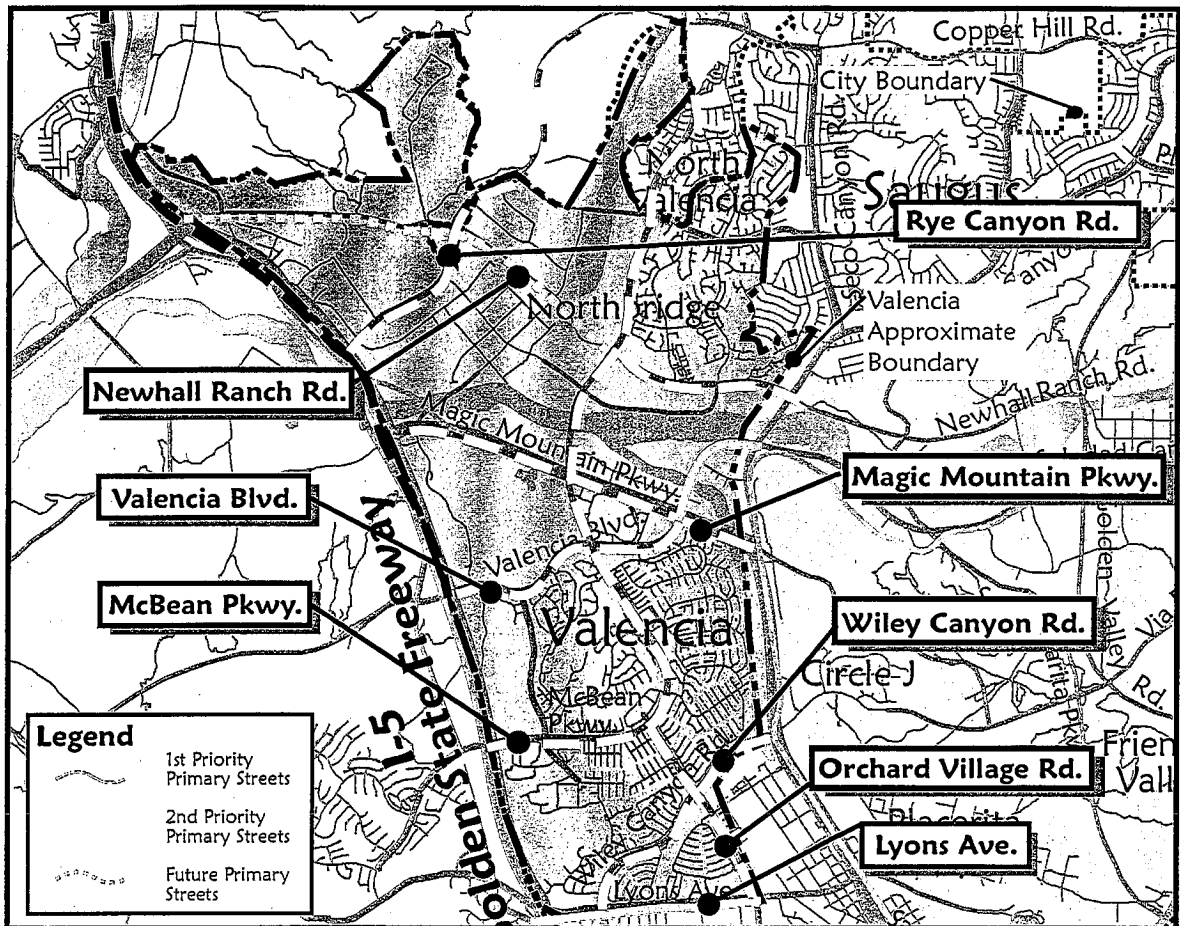
2. Beautification Needs

The beautification needs were not identified as urgent as in some other communities, however there is room for improvement in certain areas. The following list of beautification needs was generated during a key stakeholders meeting.

- Vacant lots should be screened.
- Caltrans / Los Angeles County areas should be beautified.
- Bus stops should be beautified and trash cans should be added.
- Key intersections should be designed consistently with crosswalks, landscaping, amenities and lighting.
- Blank walls should be screened with landscaping, including vines.
- Street parkways and medians should be planted and median treatments should be extended to the edges of the community.
- The Santa Clara River should be exposed and taken advantage of as an amenity.
- The amount of visual clutter including signs, overhead utilities and vehicular access points should be reduced.
- Signage should be added to unify Valencia and Santa Clarita as a whole.

Based on this community feedback, the following beautification guidelines were drafted to address the following topics. The guidelines address a range of design improvements that can lead to a more cohesive and unified look for Valencia.

- 1) Primary corridors
- 2) Signs
- 3) Medians
- 4) Street edge treatments
- 5) Fences
- 6) Community entries
- 7) Main intersections
- 8) Other beautification ideas



Valencia Primary Streets

B. Treatment of Primary Corridors

The primary corridors that extend through a given community act as the most visible and consequently the strongest unifying element within the community. Each community within the City of Santa Clarita should be clearly identifiable with distinct primary corridors that help unify it. The implementation of a community logo on each street sign, a consistent median and corridor edge treatment, a unique fence type and landscaping improvements bring together the image of a cohesive community. One of the goals of the Beautification Master Plan is to set forth long-range guidelines for improvements, enhancements and beautification of such corridors so that implementation occurs in a direction that creates a clear and easily recognizable individual community. The following streets have been designated as primary corridors within Valencia:

- First Priority Primary Corridors: Lyons Avenue and Valencia Boulevard (north of Magic Mountain Parkway)
- Second Priority Primary Corridors: Valencia Boulevard to Magic Mountain, McBean Parkway, Rye Canyon Road, Magic Mountain Parkway, Newhall Ranch Road, Wiley Canyon Road and Orchard Village Road
- Future Primary Corridors: Newhall Ranch Road



Example of a Street Sign Designed to Incorporate a Valencia Logo

1. Signs and Logo

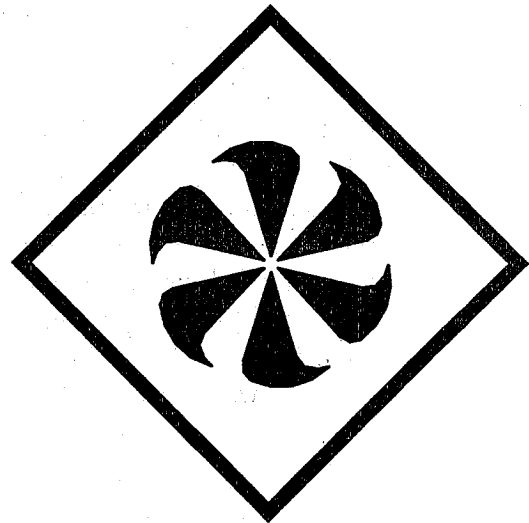
Street Sign:

The most prevalent and obvious component of the primary corridor elements is the street sign treatment. It is here that a clear and distinct demarcation occurs for each individual community. Every street sign within the community of Valencia will contain a community logo. This logo provides a recognizable sense of repetition that clearly enables a motorist, bicyclist, or pedestrian to know where they are and if they are still within the community limits. The logo will be placed within a diamond on the left side of the sign and each diamond will be a specific color for each community. This simple and quickly recognizable feature will provide a highly visible statement as to the community that an individual is in. Refer to the *Citywide Signage Program* for further information.

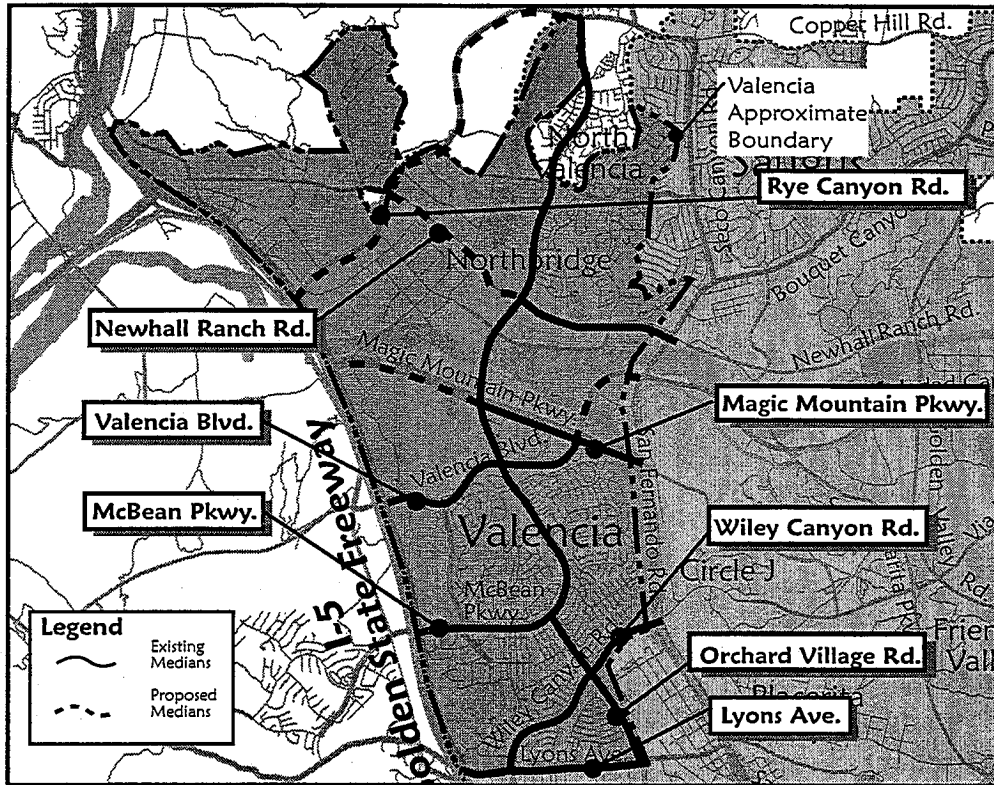
Valencia Community Logo:

Through the interview and public outreach process, participants generated a list of ideas for a Valencia logo. Of the proposed logos listed below, the majority of the participants agreed that an logo that incorporates the rotella would be most appropriate and representative of the community. Valencia is a master planned development and consequently the rotella has been used as a unifying element throughout the community for decades. If the rotella is replaced the new logo should be timeless and trendy images, colors or fonts should be avoided. Specific logo will be designed at the time of implementation, however, some possibilities include:

- Rotella
- Oranges - Town Center
- "V" with vines at the Valencia Town Center



Example of Possible Valencia Community Logo

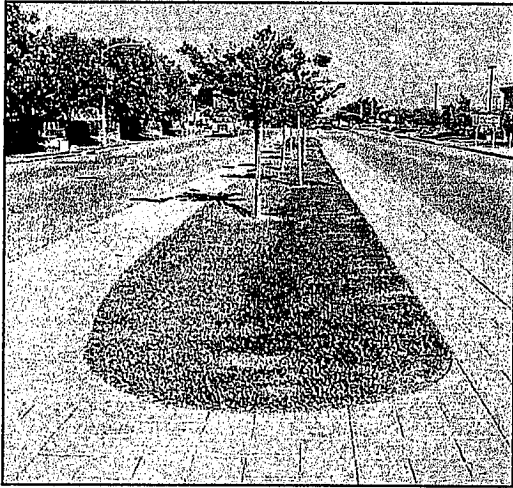


Valencia Medians Map

2. Median Treatment

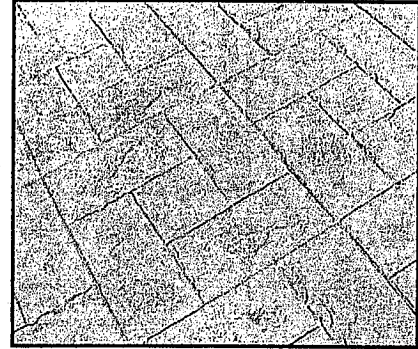
One intent of the Beautification Master Plan is to build on and extend Santa Clarita’s existing network of roadway medians into one, cohesive citywide plan that identifies each community. Valencia will have a planting theme for all medians to promote an identity within the area. The plant palette selection that will be used in the beautification and implementation of the medians will continue to enhance the adjacent landscape. The materials shall be consistent within a given community and act as a clear indicator of the difference between communities. New raised and planted medians should be considered a long-term objective for community beautification. Along with providing aesthetic appeal and consistency, the proposed medians will also control vehicle circulation, movements and serve to calm traffic. The medians will also provide a place for roadway approach street signs and directional signage at key intersections as part of the citywide signage program discussed in the *Citywide Design Guidelines*. Within Valencia and vicinity, the selected median treatment will be applied to the following roads at the locations indicated on the figure above.

- Newhall Ranch Road - from Rye Canyon Road east to existing median
- Valencia Boulevard - from the existing median east to San Fernando Road
- McBean Parkway - existing median
- Lyons Avenue - existing median
- Rye Canyon Road - from I-5 freeway north to McBean Parkway
- Magic Mountain Parkway - from the I-5 freeway east to the existing median
- Wiley Canyon Road - existing median
- Orchard Village Road - existing median



Median Treatment with Groomed Landscaping

The median treatment that has been selected for Valencia is a groomed landscaping design created with shrubs or trees and surrounded with turf. Surrounding the landscaping is an 18" safety curb that is a stamped concrete in a bromachron pattern shaped like river-side slate. The concrete is colored Valencia tan with a Goldenrod release agent.



Bromachron Pattern Safety Strip

Median Landscaping:

Trees Types which are being used

Botanical Name

- Alnus species*
- Cedrus deodar*
- Eucalyptus species*
- Fraxinus species*
- Koelreuteria paniculata*
- Lagerstroemia indica*
- Liquidambar styraciflua*
- Malus species*
- Phoenix, Washingtonia, or Brahea*
- Pinus canariensis*
- Pinus halepensis*
- Populus species*
- Prunus virginiana*
- Pyrus species*
- Quercus ilex*
- Platanus species*
- Robinia species*
- Schinus molle*
- Xylosma congestum*

Common Name

- Alder
- Deodar Cedar
- Gum Tree
- Ash
- Goldenrain Tree
- Crape Myrtle
- American Sweet Gum
- Crabapple
- Palms
- Canary Island Pine
- Aleppo Pine
- Poplar, Cottonwood
- Chokecherry
- Ornamental Pear (fruitless variety)
- Holly Oak
- Sycamore
- Locust
- California Pepper
- Shiny Xylosma

Shrubs which are being used

Botanical Name

- Acacia redolens*
- Acacia pecoff*
- Agapanthus species*
- Cercis occidentalis*
- Cotoneaster species*

Common Name

-
-
- Lily-of-the-Nile
- Western Redbud
-

Median Landscaping continued...

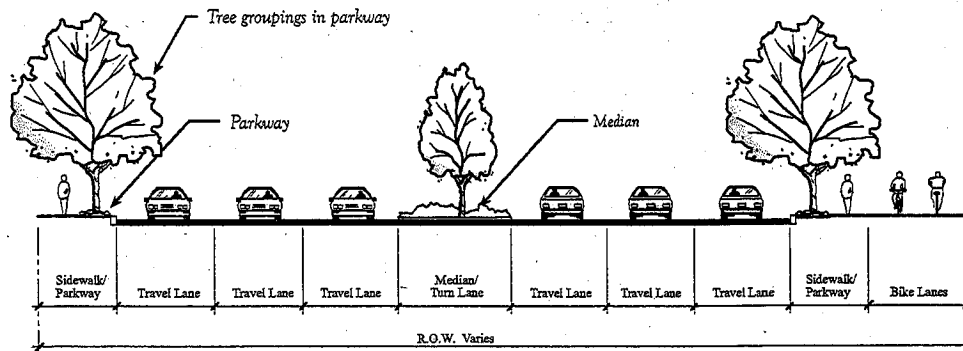
<i>Dodonaea species</i>	Hop Bush
<i>Elaeagnus species</i>	-----
<i>Escallonia species</i>	-----
<i>Euonymus species</i>	-----
<i>Hakea species</i>	-----
<i>Heteromeles arbutifolia</i>	Toyon
<i>Oleander species</i>	-----
<i>Pittosporum tobira</i> 'Variegata'	Variegated Tobira
<i>Pittosporum tobira</i>	Tobira
<i>Pyracantha</i> 'Mohave'	Firethorn
<i>Pyracantha</i> 'Teton'	Firethorn
<i>Raphiolepis indica</i> 'Enchantress'	-----
<i>Raphiolepis indica</i> 'Springtime'	-----
<i>Raphiolepis indica</i>	India Hawthorn
<i>Rosa species</i>	-----
<i>Xylosma congestum</i>	Shiny Xylosma

Groundcovers which are being used

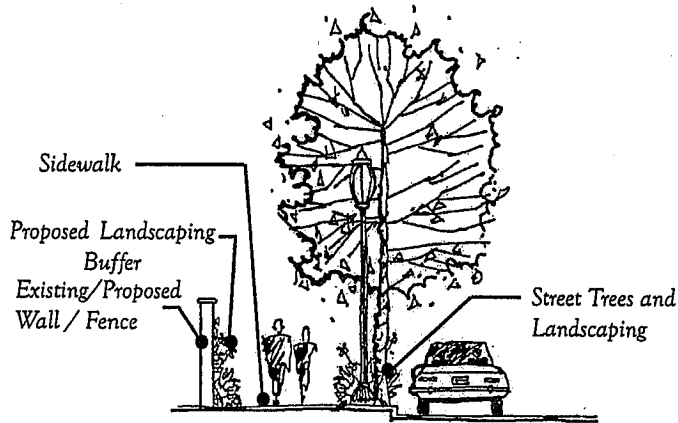
Botanical Name	Common Name
<i>Annuals</i>	-----
<i>Gazania species</i>	Trailing Gazania
<i>Hedera helix</i>	English Ivy
<i>Parthenocissus tricuspidata</i>	Boston Ivy
<i>Rosmarinus species</i>	Rosmary
<i>Turf</i>	-----
<i>Vinca species</i>	Periwinkle

3. Street Edge Treatment

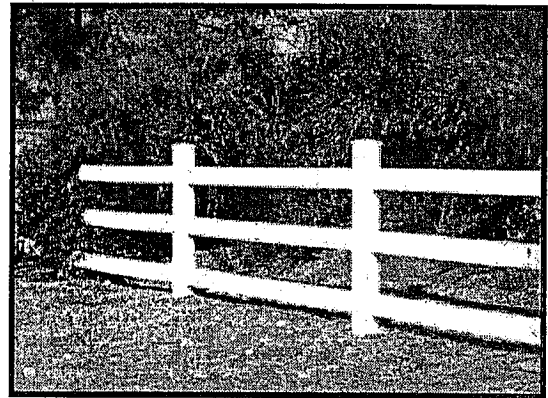
The sides of the primary corridors are just as important as the median treatment. Well landscaped parkways provide pleasant driving and walking environments while creating a sense of place. All planting shall be consistent with City Ordinance 90-15 and shall be reviewed and approved by the Director of Parks, Recreation and Community Services. A planting palette for such parkways is included in the *Citywide Landscaping Palettes* section of the document.



Example of Roadway Section



Example of Street Edge Treatment

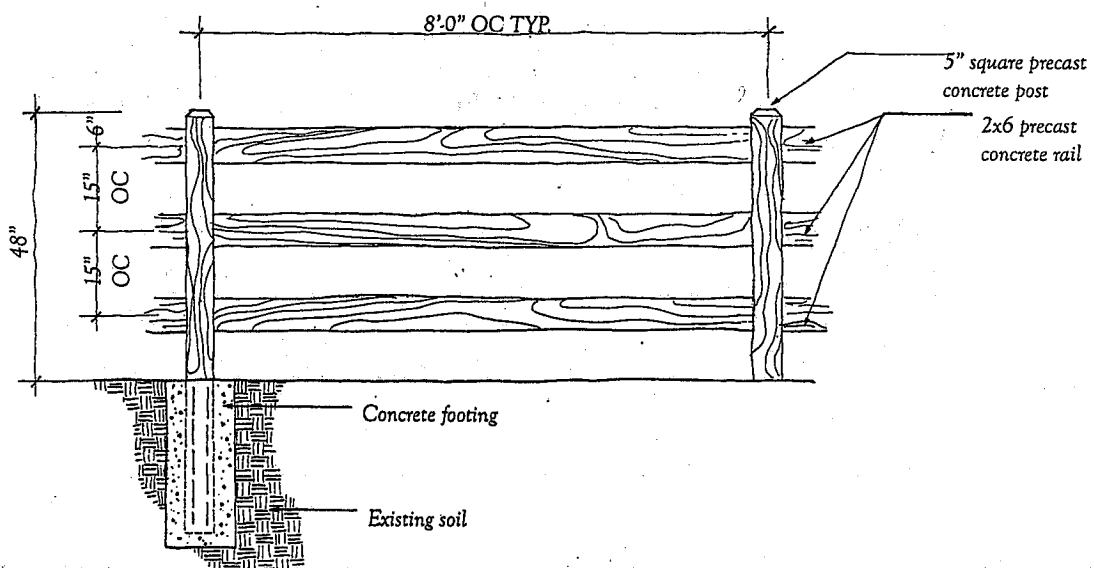


White Fence Used Throughout Valencia

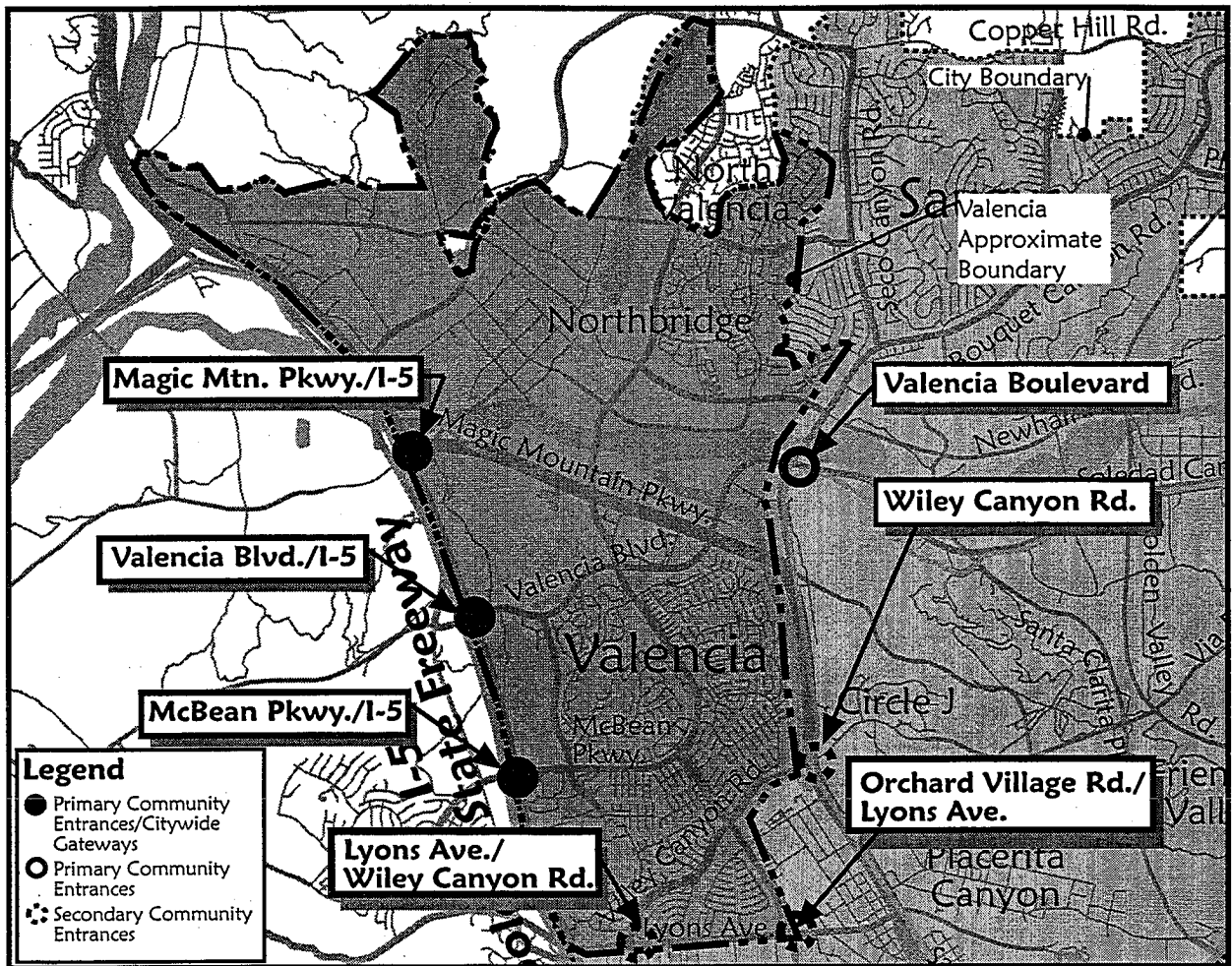
Fences:

Fences should be used to unify the City of Santa Clarita. They provide accent and help define a space and can be used to buffer parking lots adjacent to the roadway. In addition, fences used at vacant lots can “bridge” large void spaces and provide unity along the street scene. Each community will incorporate its own fencing type at intersections and along primary corridors where appropriate.

White two and three-rail fences are common throughout Valencia and have been selected as Valencia’s designated fence type. Fences are typically made of Creterail, PVC or wood. Climbing vines can be planted at fence posts to add color and texture. The clean white fence style compliments the rural yet sophisticated character of Valencia. Wood, PVC or precast concrete 2x6 rails should be used for horizontal members set into predrilled 5” square posts placed 8’ on center and set in a concrete footing for maximum stability. All fencing members should be steel reinforced, integrally colored and have a rugged wood grain texture on all sides.



Fence Post and Rail Detail



Valencia Entrances Map

C. Community Entrances and Intersections

1. Community Entrance Treatment and Locations

The entrance to Valencia should have a unique and easily identifiable “scene” that includes specific landscaping, a fence and a unique community-oriented sign. The entrances should give visitors and residents a feeling of what to expect within the community and be welcoming without being overstated. The following locations have been selected for community entrance treatments.

Primary Community Entries:

- Valencia Boulevard and Bouquet Canyon Road

Secondary Community Entries:

- Wiley Canyon Road and San Fernando Road
- Lyons Avenue and Wiley Canyon Road
- Orchard Village Road and Lyons Avenue

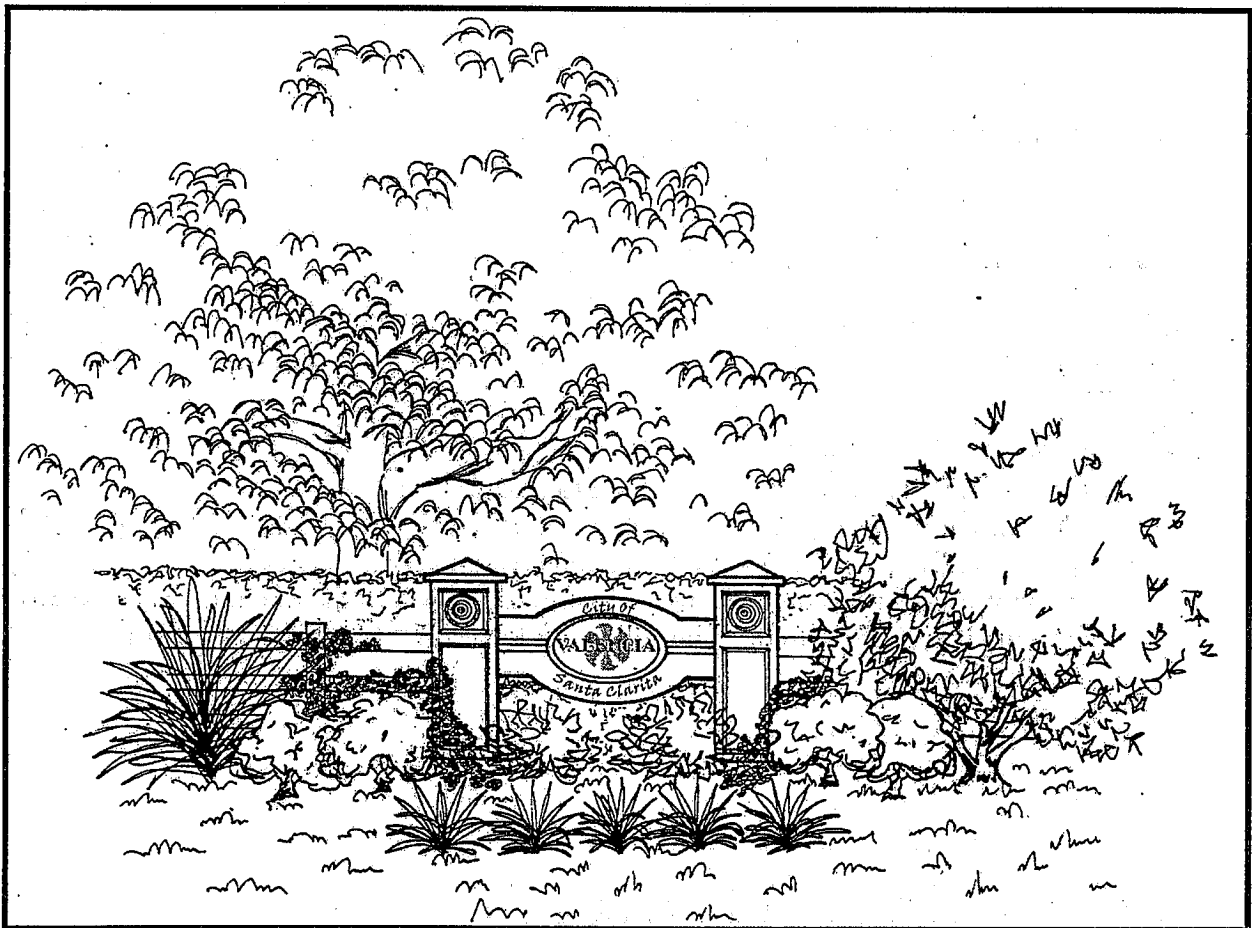
In addition to these designated community entrances, the Citywide Gateways provide excellent opportunities for community identification. Refer to the *Citywide Signage Program* section of this document for gateway treatments.

2. Community Entry Design

Community Entry Character:

The community entry is the best opportunity to reflect its individuality. The character and materials used should be a unique representation of the community. The following materials and design styles were described to reflect the character of Valencia during public meetings.

- Lush vegetation
- White three rail fence
- Urban combined with rural
- Well maintained landscaping



Proposed Community Entry Scene - Australian Willow and Grape Myrtle trees, white concrete or wood three-rail fence, special plant palette and a community-oriented sign with community logo and City name.

Community Entry Signs:

In the *Citywide Design Guidelines* section, “Entry Monuments” will help meet the goal to unify the City as a whole while the “Community Entry Signs” will help meet the other half of the goal, to embrace the different characters of individual communities. The Valencia entry sign reflects the unique natural setting and materials of the area. Community entry signs should have the following characteristics:

- Community entry signs should be located at each of the previously mentioned community entrance locations and should be coordinated with Caltrans or private landowners as appropriate. Specific locations should be determined based on visibility, right-of-way availability, sight distance criteria and minimum square footage requirements.
- Community entry signs should be lighted and landscaped.
- Community entry signs should be an integral part of the gateway scene which includes an entry sign, a fence and plants and trees approved by the Director of Parks, Recreation and Community Services.
- Community entry signs should be well proportioned and accented with landscaping. The scale of the signs should be easily legible from the road. Planting at maturity must not obscure safe vehicular sight lines.
- Community entry signs should incorporate complementary colors, materials and lettering fonts. More than one material should be used in the construction of the sign and should reflect the character of the community. The sign should be constructed for long-term durability and easy maintenance.
- All community entry signs should use the same materials, color scheme and graphic theme in order to make it very clear to visitors and residents alike that they are placed as an effort to unify Valencia.
- The community logo should appear on each sign and a community-specific font should be replicated exactly on each Valencia entry sign.
- “City of Santa Clarita” should appear on each community entry sign. The same font should be used for the citywide entry monuments.

The following landscaping palette has been created using and enhancing existing plant palettes applied throughout the Valencia area. Future planting should utilize this list or plants that are similar and compliment the desired theme.

Community Entry Landscaping

Trees

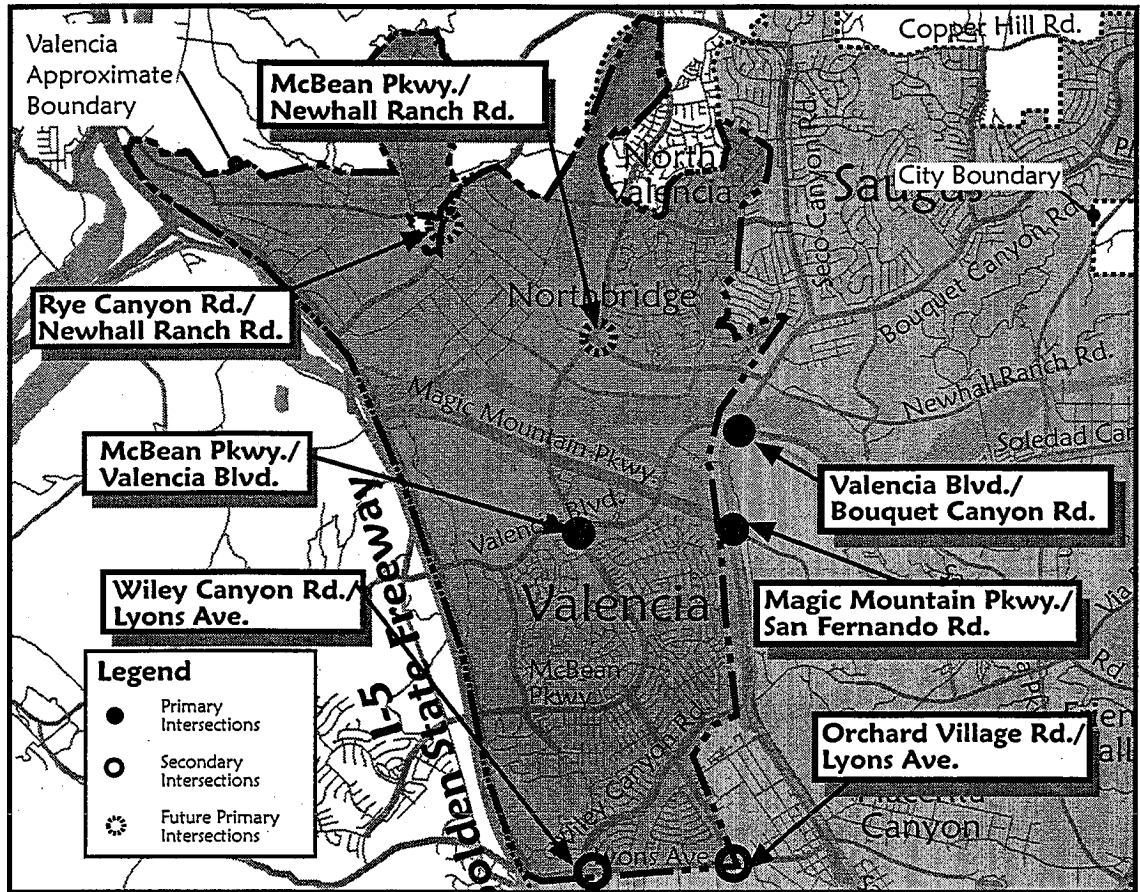
Botanical Name	Common Name
<i>Koelreuteria bipinnata</i>	Chinese Flame Tree
<i>Lagerstroemia indica</i>	Crape Myrtle
<i>Quercus lobata</i>	Valley Oak

Shrubs

Botanical Name	Common Name
<i>Dietes vegeta</i>	Fortnight Lily
<i>Hemerocallis hybrids</i>	Daylily
<i>Ligustrum j. texanum</i>	Japanese Privet
<i>Photinia fraseri</i>	Photinia
<i>Pittosporum t. Wheelers Dwarf</i>	Dwarf Tobira
<i>Raphiolepis umbellata 'Minor'</i>	India Hawthorn
<i>Xylosma Congestum</i>	Shiny Xylosma

Ground Cover

Botanical Name	Common Name
<i>Gazania 'Mitsuwa'</i>	Gazania
<i>Hedera helix</i>	English Ivy
<i>Hedera helix 'Needlepoint'</i>	Needlepoint Ivy
<i>Myoporum parvifolium 'Putah Creek'</i>	-----
<i>Vinca major</i>	Periwinkle



Key Intersections Map

3. Key Intersection Treatment and Locations

There are several intersections within the community of Valencia that stand out as opportunities to beautify and claim as unique places for Valencia. These intersections are large with multiple lane thoroughfares requiring motorists and pedestrians alike to wait for several minutes and soak in the surrounding environment. Pedestrians oftentimes have to cross over several lanes of traffic in order to travel from place to place. It is the intent of these guidelines to beautify these intersections to the extent possible and create a more safe pedestrian-friendly environment. The following key intersections have been identified as areas that need priority improvement.

Primary Intersections

- Valencia Boulevard and Bouquet Canyon Road
- McBean Parkway and Valencia Boulevard
- Magic Mountain Parkway and San Fernando Road

Secondary Intersections

- Orchard Village Road and Lyons Avenue
- Wiley Canyon Road and Lyons Avenue

Future Primary Intersections

- Rye Canyon Road and Newhall Ranch Road
- Newhall Ranch Road and McBean Parkway

Beautification of the intersections should include:

- Accent paving
- Directional signage / reduction of visual clutter
- Landscaping and street trees
- Lighting and signal pole treatment
- Underground utility poles and wires

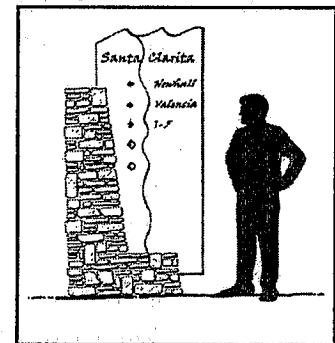
The “Bouquet Junction” intersection illustrated on the following pages depicts the advantages of implementing some of these beautification concepts.

Accent Paving:

The use of accent paving provides an opportunity to both dress up the intersection and incorporate traffic calming measures for pedestrian and bicyclist safety. The change in texture is a visual and intuitive signal to both the motorist and pedestrian of the appropriate boundaries for each. Accent paving contributes greatly to the overall visual appeal of the intersection and is typically located in one or more of the following areas: at corners; on sidewalks; at pedestrian crossings; and in the middle of intersections. At key intersections, the pedestrian crosswalks should be treated with a durable colored concrete pattern so as to visually cue the oncoming motorist to slow down because of possible pedestrian traffic. Special paving also enhances the pedestrian experience, both in visual appeal and safety. The texture should be smooth enough so that an older person, cyclist and persons with disabilities are not deterred. In addition, a surface that limits noise should be utilized. Stamped concrete in a square riverside slate pattern or real stone should be used as the accent paving throughout Valencia. This will match the median treatments in Valencia. The special paving can be throughout the entire crosswalk or just as a border.

Signage:

The directional sign described in the *Citywide Signage Program* section of the *Citywide Design Guidelines* should be placed at key intersections in order to help motorists determine the direction of the adjacent communities. It is important that existing directional signage be removed to reduce the amount of visual clutter. In addition to directional signage the removal or replacement of commercial/business signage and billboard/advertisement signs should be given first priority at key intersections.



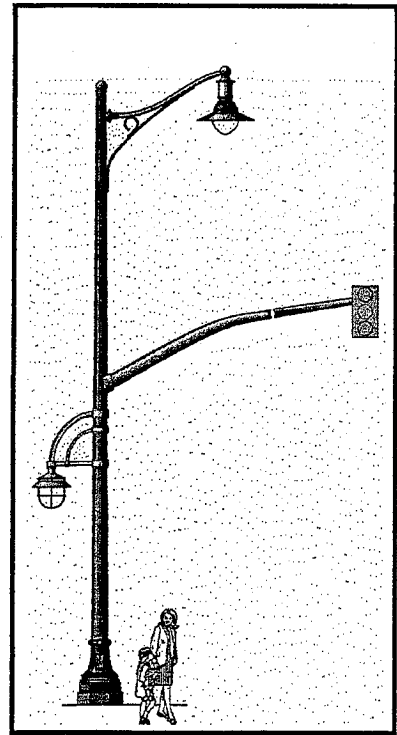
Example of Directional Signage

Landscaping Treatments:

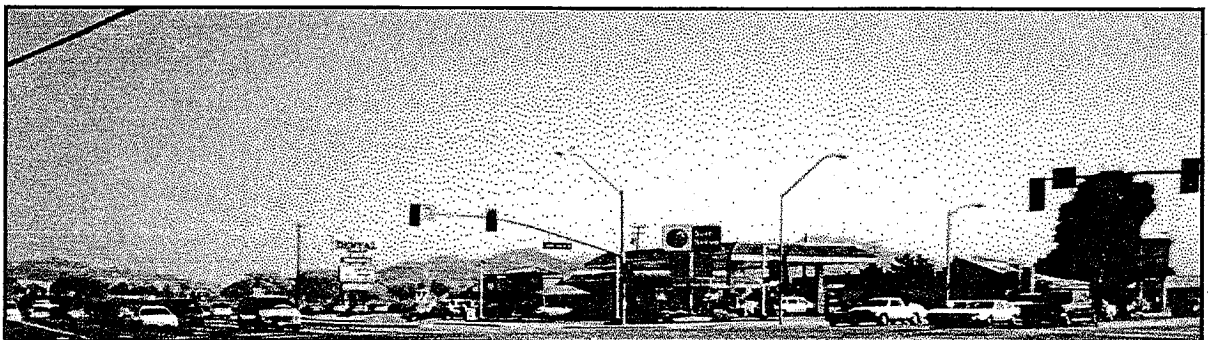
Additional vegetation located at key intersections will enhance the experience of the motorist and pedestrian. Landscaping, low walls and berms should be used to screen large expanses of paving (parking lots, service station lots) on private property. Median bull noses are 3'-0". Typical medians are 14'-0" wide and will allow for high branching trees to be planted where feasible. Trees with four-foot tree grates should be planted along the sidewalk near the intersection. The trees planted in the medians and in the parkways should correspond to those selected for a given community and primary corridor, respectively.

Lighting

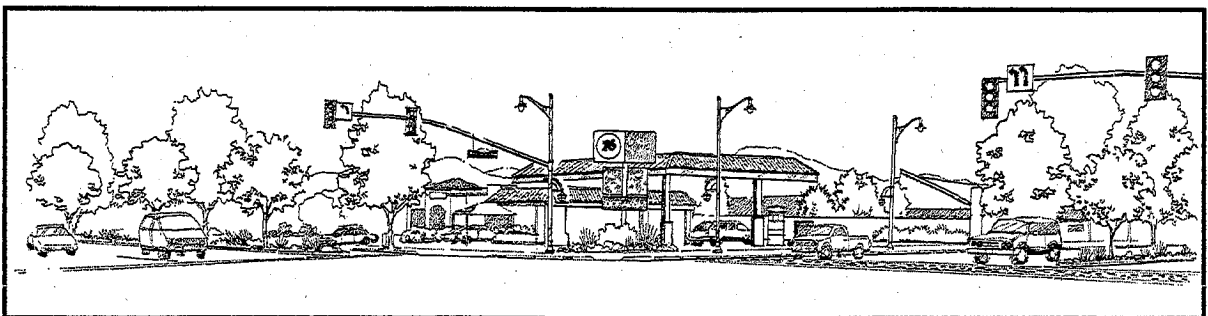
The implementation of a uniquely designed street light and signal poles at key intersection can enhance the experience of the pedestrian and motorist. A minor improvement involving the addition of an ornamental base and fixture to the existing pole can greatly improve its aesthetic appeal. All specially designed street lighting and signal poles shall be reviewed and approved by the appropriate agencies.



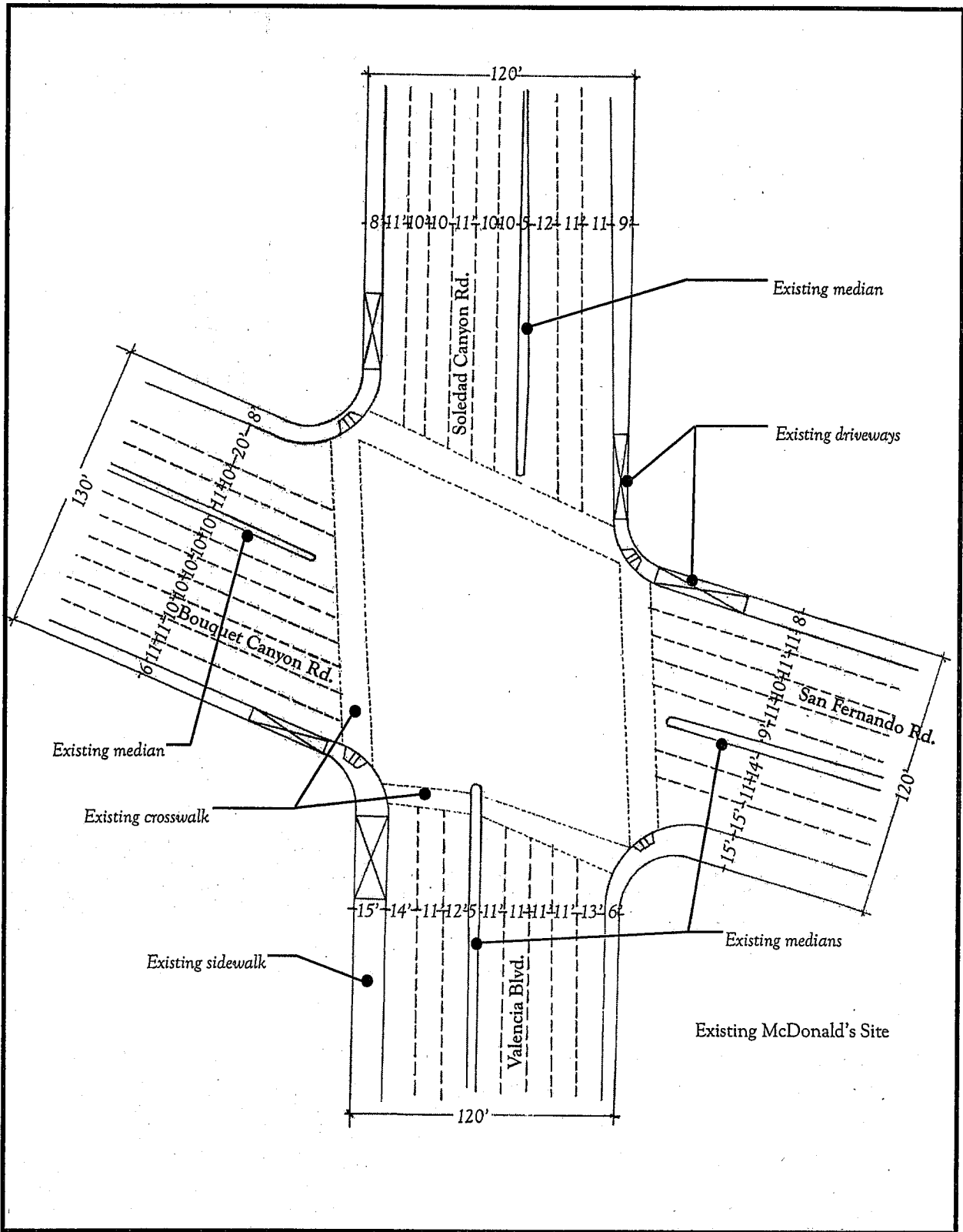
Example of Specially Designed Street Light and Signal Pole Enhancements



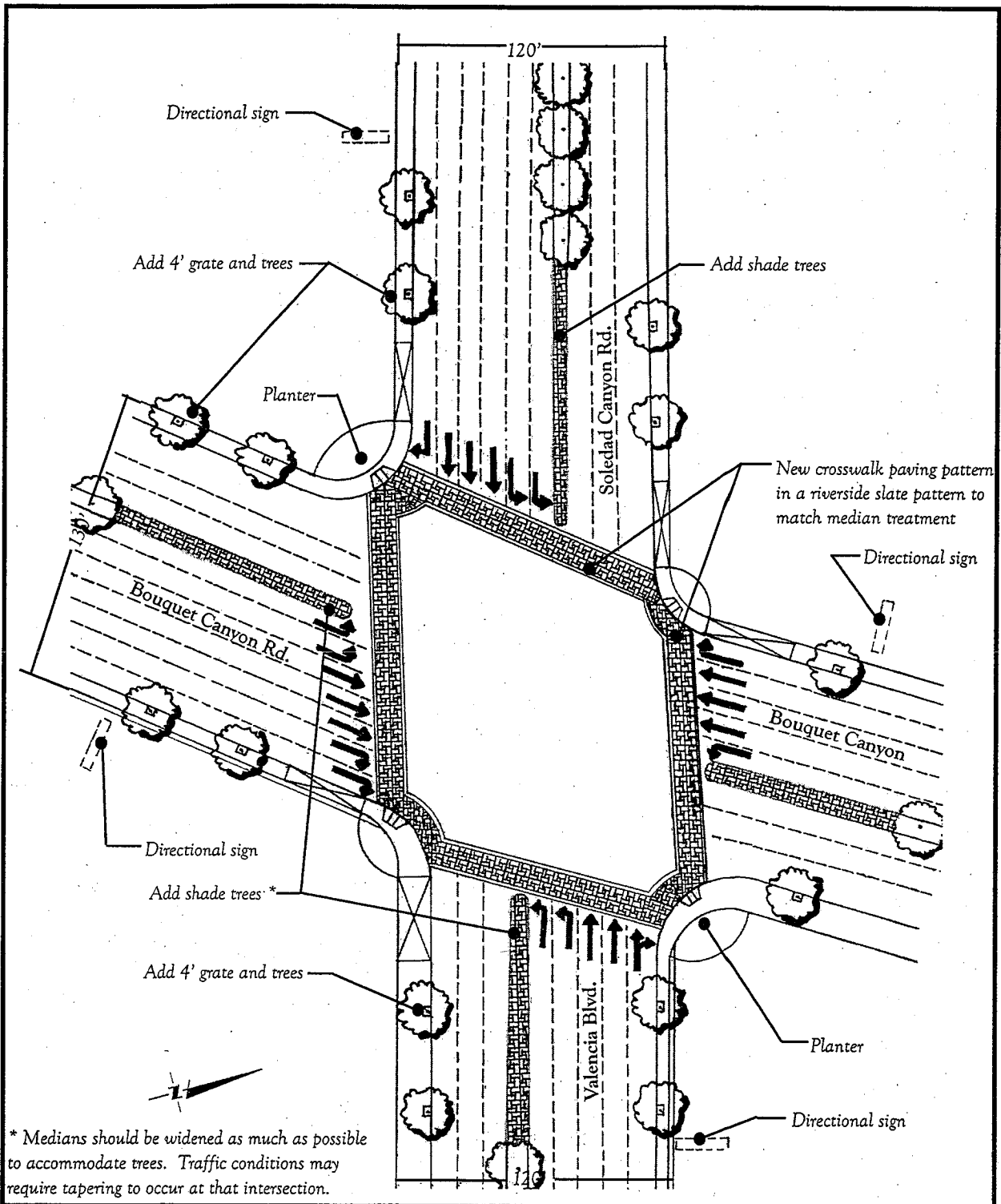
Before: "Bouquet Junction" Intersection



After: "Bouquet Junction" Intersection



Before: "Bouquet Junction" Intersection



After: "Bouquet Junction" Intersection

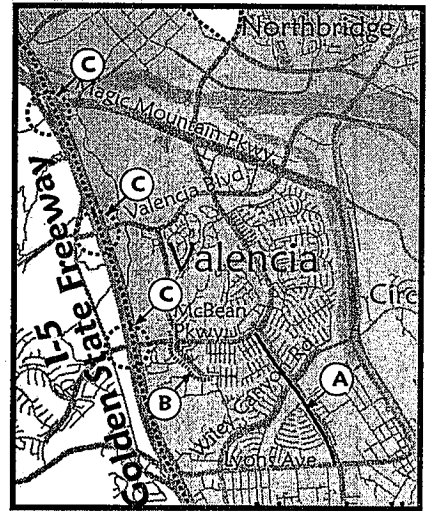
D. Other Beautification Ideas

This section addresses the beautification needs identified by participants from various workshops and provides suggestions for additional ways in which a community might encourage beautification efforts.

1. Specific Beautification Needs

The following beautification needs are not directly addressed by the Beautification Master Plan, but are areas that community members have identified as issues needing improvement.

- A. Edison easement should be improved, especially at Orchard Village Road.
- B. The portion of the block wall on Tournament Road that has been replaced with a wrought iron fence should be consistent with the rest of the street.
- C. Exits at freeway off ramps need color, variety, landscaping and provisions for maintenance.



Valencia Beautification Needs

2. Private Property Treatment

To assist city beautification efforts within the street right-of-way, private property owners should strive to accomplish the following:

- Reduce sign clutter
- Landscape parking lots and building frontage
- Screen unsightly storage and trash areas
- Coordinate wall/fence treatment between properties
- Maintain vacant lots free of trash and weeds
- Seek to upgrade building facades consistent with design image
- Plant trees to add shade and soften urban look
- Use fencing and landscaping along roadways at vacant parcels

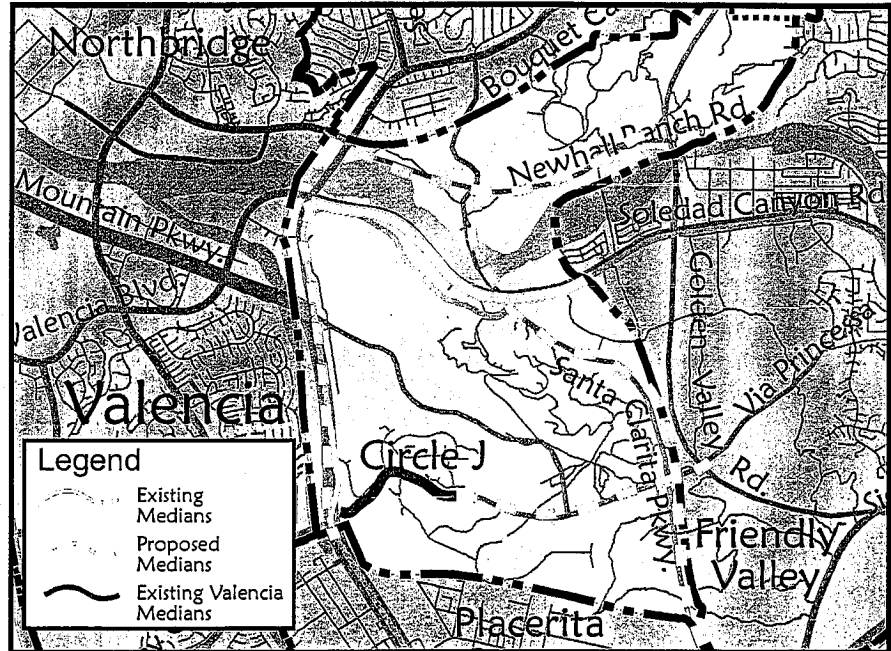
3. Getting Involved In The Process

Community groups should make themselves aware of the City design review and approval process. Likewise, the City should notice a member of the organization of upcoming meetings and proposed projects. This established communication will help to ensure that projects are approved that residents and City staff are proud of. In addition, community groups might take on banner projects such as organizing and encouraging private property owners to improve their building facades or plant vines in front of unsightly walls and fences.

**CENTER
CITY**

VI. Center City Median Treatment

One intent of the Beautification Master Plan is to build on and extend Santa Clarita's existing network of roadway medians into one, cohesive citywide plan. The area located at the center of the City will provide an opportunity to create a unique median treatment to indicate where all of the four communities intersect. New raised and



Center City Medians Map

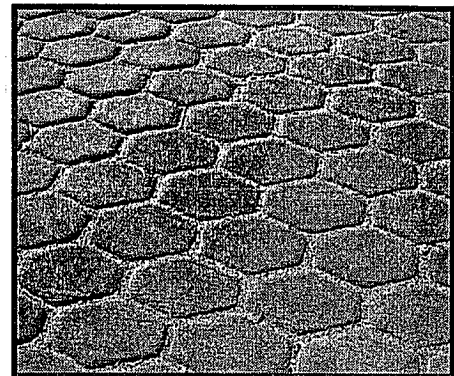
planted medians within this area should be considered a long-term objective for citywide beautification. Along with providing aesthetic appeal and consistency, the proposed medians will also control vehicle circulation, movements and serve to calm traffic. The medians will also provide a place for roadway approach street signs and directional signage at key intersections as part of the regional monumentation program discussed in the *Citywide Design Guidelines*. Within the Center City area, the selected median treatment will be applied to the following roads at the locations indicated on the figure above.

- **Via Princessa** - from the existing median east to Golden Valley Road
- **Santa Clarita Parkway** - Newhall Ranch Road south to Highway 14
- **Soledad Canyon Road** - existing median
- **San Fernando Road** - from Soledad Canyon Road to Wiley Canyon Road
- **Newhall Ranch Road** - from Bouquet Canyon Road east to Golden Valley Road

Currently median maintenance is funded by either City General funds or privately funded Landscape Maintenance Districts. These different funding sources, provide opportunities for different median design concepts. Both designs should incorporate a similar plant palette that will help to enhance and unify the community. A drought tolerant design which offers a lower maintenance solution with plants that have an ability to thrive in tough conditions and require less water has been used in the area and future medians should continue to incorporate this type of planting palette. A combination of crescent shaped landscaped pockets and stamped concrete paving that is designed to embrace a Mediterranean theme should continue to be used, however the exact colors and materials will be determined during the implementation phase.



Median Treatment with Xeriscape Landscaping



Stamped Concrete stamped in a hexagonal shape

The median treatment that has been selected for the Center City area is a drought tolerant landscaping design that incorporates a drip irrigation system. The landscaping is surrounded with a 18 inch safety strip that is a stamped concrete shaped like hexagons with Mediterranean colors which will be determined later.

Median Landscaping

Trees Types that are being used

Botanical Name

Geijera parviflora

Lagerstroemia indica

Olea europaea

Quercus agrifolia

Common Name

Australian Willow

Crape myrtle

Olive

Coast live oak

Shrubs which are being used

Botanical Name

Rosmarinus officinalis 'prostratus'

Cotoneaster 'lowfast'

Echium fastuosum

Dietes vegeta

Myoporum parvifolium

Phormium tenax 'Maori Chief'

Common Name

Prostrate rosemary

Cotoneaster

Pride of Madeira

Fortnight Lily

New Zealand Flax

IMPLEMENTATION PLAN

Implementation Plan

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VII. Implementation Plan

A. Introduction

One of the most important elements of the Master Plan is this chapter on implementation. It clearly lays out a "road map" to implement the design recommendations. Improvements identified in the Master Plan are fairly complex due to the number of roadways and beautification elements. The project also has overlapping adjacent jurisdictions which will require coordination, including:

- L.A. County which surrounds the City of Santa Clarita.
- Caltrans jurisdiction over the freeway and on/off ramps.

The improvement recommendations have been broken into approximately 22 individual projects. They have been organized in the same manner as the Beautification Master Plan, beginning with the citywide improvements followed by community improvements, and finishing with special projects. The objective of this implementation plan is to identify potential funding sources, identify beautification projects, provide budget estimates for projects and prioritize the recommended improvements identified in the Plan.

B. Discussion of Potential Funding Sources

The Beautification Master Plan contains recommendations for a variety of project types. As such, a variety of funding sources are available to implement these recommendations. The following describes some of the various funding mechanisms that are currently available at the local, State and Federal level.

1. Local Funding Sources

General Fund Monies:

This is the most accessible and flexible funding source available to local agencies. It is money derived from local revenues such as property tax and transient occupancy tax and expended on projects and programs as defined in an adopted Capital Improvement Program. Projects and programs that may be funded by this source of money generally include those items which cannot be paid for by other sources of funding and which provide a direct community-wide benefit. However, since this funding source is extremely limited and highly competitive, it should be considered as a secondary source of funds for most projects.

Gas Tax Revenue:

The City receives State Gas Tax revenue, which can be used for transportation planning and capital improvements for roadways. Gas tax is based on the percentage of total value of gas sold within the City.

Property and Business Improvement Districts (PBID):

A property and business improvement district is a benefit assessment district that is available through legislation known as AB3754. PBID districts are geographically defined business areas in which private property owners have banded together to gain legal standing and sufficient

revenue to realize common goals to improve the service and facilities of an area. All types of businesses can be included with this district, including commercial, professional office, finance institutions, and higher density residential. The district can perform a number of activities designed to supplement existing City services such as marketing the district businesses and activities, promoting public events, street and sidewalk cleaning, graffiti removal, tourism, sanitation, retail retention and recruitment. Physical improvements such as benches, kiosks, pedestrian shelters, signs, lighting, restrooms, trash receptacles, planting area, fountains, and plazas, among other features, can be funded by the PBID.

Municipal Bond Financing:

Federal and State laws allow cities to issue bonds with interest payment to investors that are exempt from Federal and State income, thus allowing cities to sell the bonds at below market interest rates. Cities in turn can utilize funds for certain projects that serve a public purpose. While this may limit the opportunity to use municipal revenue bonds, these bonds still remain a very powerful vehicle for financing capital improvements. Bond payments for improvements would be secured by the formation of an assessment district. An assessment district such as a landscape, lighting or street improvement can be formed to fund public improvements that will benefit a localized area. The City floats bonds to pay for such improvements. The debt is paid by assessing property owners who will be served by this improvement. The individual property owner portion of the debt is based on the owner's proportion of benefit. Any method that reasonably measures these benefits can be used to spread the debt among property owners. One drawback of municipal bond financing is that it requires approval of two-thirds of the voters in a local election.

Landscape and Lighting Districts:

The City of Santa Clarita has a number of existing landscape and lighting maintenance districts. These districts were established to maintain landscaping and lighting in sub-areas, the maintenance cost being paid for by assessments on property owners within each district. These existing districts could be expanded to provide funds for maintenance and capital expenditures for new improvements by annexation of additional area. In addition, new districts may be created as development takes place for improvements and maintenance of future projects or for improvements and maintenance of existing areas.

Development Impact Fees:

Many of the projects identified in the plan are necessary because of the pressures of new development and growth in the area. Therefore, it is logical to attach the financial responsibility of some of these improvements to new development. A mechanism commonly utilized for funding roadway improvements is development impact fees. Impact fees collected through this mechanism are based on proportion of impact relative to improvements necessary, providing a clear connection or "nexus" between development and project improvements. For example, roadway enhancements, stamped concrete crosswalks and intersections, medians and bus turnouts can be improvements directly related to projects adjacent to the roadway. Intersection improvements, roadway widenings, bikeways adjacent to new roads, and medians funded are fully funded through Bridge and Thoroughfare fees.

2. State Funding Sources

Metropolitan Transportation Authority Proposition A and C Funds:

Proposition A and C programs are the ½ cent sales tax measures approved by Los Angeles County voters in 1980 and 1990 respectively. Monies from these tax funds may be used in the following ways:

- *Proposition A Local Return Funds:* Used for public transit including fixed route and para transit, transportation system management and fare subsidy. Proposition A funds can also be traded for other cities general funds.
- *Proposition C Local Return Funds:* May be used for public transit projects as described above as well as a broader category of public transit, bikeways, road improvements that benefit transit and congestion management activities. Proposition C funds cannot be traded.

Metropolitan Transportation Authority Transportation Improvement Program:

The Los Angeles County Metropolitan Transportation Authority (MTA) is required by Federal and State statutes to prepare a Transportation Improvement Program (TIP) for Los Angeles County. The MTA is required to program revenues in the TIP across all transportation modes. Biannually, on the odd numbered years, the MTA TIP Call for Projects programs a variety of local, State, and Federal revenues to regionally significant projects from several transportation modes throughout the County.

To take advantage of the flexibility of various funding sources, the MTA has developed a single unified process, which allows each applicant to apply for specific regionally significant projects of interest to the applicant agency without regard to funding source. Through the Call for Projects, the MTA identifies needs and allots an amount of funds to various regional transportation projects. Once needs are identified and projects are selected, specific funds are assigned to meet the eligibility requirements of funding sources and to leverage the maximum amount of Federal and State funds for the County.

Projects identified in the Beautification Master Plan could compete for funding in the following modal categories:

- Transportation Demand Management
- Bikeway Improvements
- Pedestrian Improvements
- Transit Capital (bus stop improvements)
- Transportation Enhancement Activities (roadway landscaping)

The applications are evaluated by the MTA using the following criteria:

- Regional Significance, Project Benefit, and Intermodal Integration
- Cost Effectiveness and Local Match
- Land Use and Environmental Compatibility
- Benefit to Transit System
- Project Need, Long-term Project Development, and Management of Existing Systems
- Project Readiness

State of California Department of Transportation Minor Capital Program:

Funding may be available for State highway projects, including interchanges, median treatments, on- and off-ramps, and landscaping.

Petroleum Violation Escrow Account (PVEA):

This program is funded by fines levied against petroleum producers in the State, and is available to local jurisdictions for projects which demonstrate energy conservation, such as bicycle and pedestrian facilities. Project funding must be approved by Caltrans or by special legislation for allocation to local agencies, and subject to review by the California Energy Commission and U.S. Department of Energy.

Environmental Enhancement and Mitigation Fund (EE&M):

EE&M monies are another State funding source. The EE&M fund is affiliated with the State Highway account fund. The EE&M funds are set aside for environmental enhancement and are available through a competitive process to various local agencies throughout the State. Eligible projects are those which contain environmental elements which will serve to beautify or environmentally enhance a roadway. Such elements may include landscaping for heat reduction and traffic calming, rubberized asphalt for noise reduction, and drainage and runoff systems to help meet NPDES standards.

Office of Traffic Safety (OTS):

The California Office of Traffic Safety (OTS) is charged with reducing fatalities, injuries and economic losses resulting from motor vehicle crashes through the administration of the California Highway Safety Plan (HSP). This program is available to local jurisdictions for safety program implementation and training, and for identification of highway hazards. The program requires a 25% match.

3. Federal Funding Sources

Metropolitan Transportation Authority Transportation Improvement Program:

Many Federal funds are available to the City through the competitive MTA TIP program previously described.

Community Development Block Grant Funding (CDBG):

Community development block grant money is widely distributed to various entitlement communities throughout the country. Block grants are used for a variety of community development projects and can be allocated toward funding various roadway improvements that focus on eliminating blight and revitalizing economic conditions in local communities.

Transportation Equity Act for the 21st Century (TEA-21):

This Federal funding program provides money for transportation enhancement activity such as bike lanes, landscaping, beautification, safety and assistance in alternative transportation beautification activities. Improvements such as street trees, sidewalks, pedestrian crossings, bike lanes, median landscaping, street repair, intersection signalization and transit improvements all may be eligible for funding. Caltrans is the agency responsible for allocating these

funds in California.

TEA-21, which replaced ISTEA, authorizes continued funding for highway, transit, and transportation safety programs through 2003. The Federal act provides funding for two major areas that include highways and transit. There are six main funding categories in the highways program. They are the Interstate Maintenance, National Highway System, Congestion Mitigation/Air Quality Improvement (CMAQ), Surface Transportation Program (STP), Bridges, and Minimum Guarantee. As under ISTEA, the transit program continues to include Urban Formula, Fixed Guideway (rail) Modernization, Rail New Starts, and the Bus/Bus Facility funding categories.

Transportation Enhancement Activities (TEA):

This competitive program is available to local jurisdictions for projects which enhance the transportation environment, including bikeway and streetscape projects. This program is administered locally by the Los Angeles Metropolitan Transportation Authority and approved statewide by the California Transportation Commission. It requires a 20% match from the local jurisdiction.

U.S. Department of Agricultural Forest Service Fund:

The USDA Forest Service provides funds for a variety of urban and community forest programs. In addition, the Forest Service provides information on selecting, planting and maintaining trees in stressful urban environments.

C. Citywide Improvement Projects

The following is a list of improvement projects and the estimated construction budget for each. An additional 15% of construction costs should be added for design fees on any project that would require design refinement and construction documents. These budgets were estimated in 2001. A 3% cost escalation increase should be estimated per year for standard inflation / construction cost adjustment.

1. Signage Program

Sign Design Consultant:

- Description: A signage consultant would refine the direction received from the community workshops, as outlined in the Beautification Master Plan. This would include specifying the colors, fonts, sizes, materials and text layout for gateways, community entrances, directional monuments, street signs, roadway approach signs, trailhead monuments and markers, and other standard City signs (destination signs, civic building signs, bus, transit, and parking). This would also include developing four community logos with public outreach and a budget estimate for sign types.
- Assumptions: The sign consultant would work with the City and/or its consultant to finalize the design direction.
 - Quantity: N/A
 - Estimated Budget: \$32,000 - \$48,000
 - Exclusions: City and/or its consultant's time to coordinate the final design and how it works with the Beautification Master Plan. Construction documents for signs and specifications for sign fabrication would be completed prior to implementation as phase two with the sign consultant.

Gateways:

- Description: Gateways are intended to identify primary entrances into the City of Santa Clarita. These gateways should include an entry monument sign of natural materials, Bouquet Canyon Stone, a three-rail fence, lighting (if possible), Live Oaks and Cottonwoods, and landscaping.
- Assumptions: There are twelve (12) locations (six (6) primary, four (4) secondary, and two (2) future) with gateway scenes located at each of the north and southbound off-ramps for a total of 24 gateway scenes. If more than one scene were constructed at a time, the cost would decrease.
 - Quantity: 24
 - Estimated Budget: \$20,500 each
 - Exclusions: Land acquisition, earth movement, site preparation, processing, design and environmental review.

Directional Monuments:

- Description: Directional monuments are intended to identify how to find specific destinations within the City (communities, parks, post office ...). These monuments are smaller versions of the entry monument signs at the City gateways. These signs will be located at key intersections and should include a monument sign of natural materials, lighting (if possible), Bouquet Canyon Stone, and landscaping.
- Assumptions: Locating four (4) monuments at each key intersection. There are twenty-one (21) locations, (ten (10) primary, five (5) secondary, and six (6) future)
 - Quantity: 84
 - Estimated Budget: \$8,200 each (minimum construction scope - four monuments)
 - Exclusions: Land acquisitions, earth movement, site preparation processing, design and environmental review.

Open Space Trailhead Monuments:

- Description: Open space trailhead monuments are intended to create a sense of arrival and unify the City signage. These monuments are smaller versions of directional monument signs and will be located at key access points to the pedestrian and bicycle trail system. Each trailhead monument should include a monument sign of natural materials, trail map, Federal recreational symbols (identifying allowable activities i.e. picnic areas, bicycles ...), lighting (if possible), Bouquet Canyon Stone, and landscaping.
- Assumptions: This would require planning for the appropriate symbols and locations prior to construction.
 - Quantity: Unknown
 - Estimated Budget: \$3,800 each (minimum construction scope - four monuments)
 - Exclusions: Land acquisition, earth movement, site preparation, processing, design and environmental review.

Trail/Open Space Markers:

- Description: Trail/open space markers will be used as mile markers, helping people measure their distance traveled while incorporating another opportunity to unify the City signage program. These monuments could be bollard style, and made of concrete with a mile marker and a City logo. They will be located along the pedestrian and bicycle trail system at mile increments.
- Assumptions: The cost of this could be lowered if design is simplified.
 - Quantity: Unknown
 - Estimated Budget: \$950 each (minimum construction scope - four markers)
 - Exclusions: Land acquisitions, earth movement, site preparation, processing, design and environmental review.

Street Signs:

- **Description:** These are the signs that will identify local streets and are located at street corners. There will be four versions of signs, one for each of the four communities, Valencia, Saugus, Newhall, and Canyon Country. All the signs should be the same shape, use the same fonts, and incorporate the City logo, but signs within each community should be a community specific color and should include the individual community logo. The sign posts could have the optional finial attachment on top.
- **Assumptions:** Development of community logos needs to occur prior this task (this cost is included in Section IV.C.1). Existing sign posts and attachments can be reused.
 - **Quantity:** Unknown. Sign planning (by City or consultants) for phasing the installation of street signs would determine quantities/schedule (high visibility arterial streets or neighborhood-by-neighborhood).
 - **Estimated Budget:** \$35 - \$95 per sign blade
 - **Exclusions:** City labor to replace signs, community logos design process, design refinement, site preparation, and hardware to attach signs to existing posts.

Roadway Approach Signs:

- **Description:** Roadway approach signs are located in medians to notify motorists of the upcoming street. There are two options to this sign, a simple option with sign poles and finials, and a more substantial option with stone. These signs will also include the City logo and the community logo.
- **Assumptions:** A series of signs will be implemented in one consolidated effort (implementing these one or two at a time will escalate the cost significantly). Planning to be conducted by a qualified architect or sign consultant using Manual of Uniform Traffic Control Devices roadway guidelines for setback and site line view corridors.
 - **Quantity:** Unknown
 - **Estimated Budget:** Concrete/stone: \$2,800 - \$4,500 each
Reflectorized vinyl/aluminum/posts: \$450 - \$1,200 each
Porcelain enamel on steel/wood frames/posts: \$950 to \$2,500 each
 - **Exclusions:** Site preparation, processing, design and environmental review.

Consolidation of Public Signage:

- **Description:** This will include installing a sign framework that can hold a number of different signs on one pole. This may mean that the individual signs will have to be resized to fit into this framework. Each framework will include the City logo with the option of a finial.
- **Assumptions:** The City would coordinate, develop, and process specific information for multiple-agencies (bus, train ...).
 - **Quantity:** Unknown
 - **Estimated Budget:** Reflectorized vinyl/aluminum/posts: \$950 - \$1,250 each
Porcelain enamel on steel/aluminum frames and post: \$2,550 - \$3,550 each
High-pressure laminated digital color output: \$950 - \$1,550 each
 - **Exclusions:** Site preparation, processing, environmental design, City labor, and coordinating time for sign removal and consolidation.

2. Streetscape Beautification

Freeways:

- Description: This includes landscaping the rights-of-way of Interstate 5 (Golden State Freeway, I-5) and State Route 14 (Antelope Valley Freeway, SR 14).
- Assumptions: Twenty-five (25) feet deep on each side of freeway; shrub planting in median areas where feasible.
 - Quantity: 83,530 linear feet of freeway (167,060 linear feet if both north and southbound sides are improved); one acre = 1,742 linear feet at 25 feet wide.
 - Estimated Budget: \$165,650 per acre (minimum construction scope - one acre)
 - Exclusions: Site preparation, processing, environmental design, City labor, and negotiations with Caltrans.

Freeway Off-Ramps:

- Description: This includes landscaping areas along the off-ramps of Interstate 5 (Golden State Freeway, I-5) and State Route 14 (Antelope Valley Freeway, SR 14). Any area that is highly visible will need to be landscaped.
- Assumptions: Twenty-five (25) feet deep on each side of off-ramp.
 - Quantity: Unknown
 - Estimated Budget: \$283,250 per acre (minimum construction scope - one acre)
 - Exclusions: Site preparation, processing, environmental review, negotiations with Caltrans, design and City labor.

3. Citywide Unification Elements

Open Space and MTA Connectors:

- Description: This includes improving existing and creating new bicycle and pedestrian trails and paths along MTA rights-of-way and river corridors.
- Assumptions: A 12-foot wide unpaved path with three-rail fence on one side, and 10 feet of landscaping on each side. The cost of open space markers and trailhead monuments are included in the signage program. Updating existing trails would include adding a fence and landscaping.
 - Quantity: Unknown
 - Estimated Budget: \$588,700/linear mile of new trails; \$462,000/linear mile of updated trails
 - Exclusions: Site preparation, processing, environmental design, and City labor.

Bus Shelters/Benches and Trash Cans:

- Description: This includes bus shelter structures, benches and trash cans without advertisement. There are currently 68 shelters and 247 stops that would need to be replaced plus any additional stops/shelters for future needs.
- Assumptions: Some shelters will be provided by new development; concrete pad for shelter included in costs.
 - Quantity: 68 shelters, 247 benches and 47 trash cans
 - Estimated Budget: \$6,800/pre-fab metal bus shelter with seating and trash cans;

\$10,000 - \$20,000/stone and wood bus shelter with seating and trash cans;
or \$1,300/bus stop with only one bench and one trash can pre-fab (lighted).

- Exclusions: Site preparation, processing, environmental design, and City labor.

4. Utility Lines (Underground)

- Description: This includes under-grounding all utility lines along primary and secondary corridors.
- Assumptions: The City will periodically underground sections of roadways as grant money allows, however these grants cannot finance all work.
 - Quantity: Unknown
 - Estimated Budget: Approximately \$1 million/linear mile for lines below 66KV and \$3 million/ linear mile for higher voltage.
 - Exclusions: City staff time, underground district formation, public outreach, property owner negotiations, processing, easement acquisitions, environmental and design plans.

5. Screening of Vacant Lots

Identify Blighted Areas/Vacant Lots:

- Description: This includes hiring a consultant to identify areas that are blighted or vacant that have a big impact on the beautification effort of Santa Clarita. The goal would be to find key parcels that with a little vegetation and a temporary fence could be cleaned up. This would include site research, property line research, and prioritizing the sites.
- Assumptions: This would incorporate primary corridors only.
 - Quantity: Consultant or staff costs.
 - Estimated Budget: \$15,000 - \$25,000
 - Exclusions: Public outreach, property owner negotiations, processing, easement acquisitions, environmental and design plans.

Beautify Key Vacant Parcels with Temporary Fences and Landscaping:

- Description: This includes planting a 10-foot wide area along a vacant parcel on a primary corridor and incorporating a temporary three-rail fence.
- Assumptions: Ten-foot wide of planting and a three-rail fence. This assumes there is existing parkway landscaping adjacent to parcels with access to irrigation mainline.
 - Quantity: Unknown
 - Estimated Budget: \$7,100/100 linear feet
 - Exclusions: Processing, easement acquisitions, environmental review, earth movement, site preparation, cost of adding water meters, and design.

D. Community Improvement Projects (Canyon Country, Newhall, Saugus, and Valencia)

1. Streetscape Beatification

In areas where the future streets will be built by developers, the landscaping in the medians and parkways will be installed by the developers. In most of these cases, a landscape maintenance district will be established to fund long term landscape maintenance.

There are existing areas that are proposed to have medians constructed and landscaping installed in the future. These existing area may need to have the maintenance of the new landscape funded through a district. In these areas where a landscape district is proposed to fund long term maintenance, residents and property owners may be required to form a district prior to the installation of the landscaping. (Some median areas along flood control channels or along the river do not have specific benefit to adjacent properties and it may not be possible for these areas to be maintained through a landscape maintenance district) Whenever possible landscape districts should be formed to provide long term funding for the landscape maintenance.

Primary Streets' Medians and Street Edges:

- Description: This would include the primary streets listed in the Beautification Master Plan that do not yet have landscaping at street edges or medians. The medians include stamped concrete safety edge treatment, natural stones, landscaping and trees. The street edge treatment include landscaping, three-rail fence, where appropriate, and street trees.
- Assumptions: Includes both sides of the street.
 - Quantity: Unknown
 - Estimated Budget: Unknown
 - Exclusions: Areas constructed by developers

2. Community Entrances

Develop Community Logos:

- Description: This would include hiring a consultant to work with each of the four (4) communities and develop a logo for each community that would be incorporated on the street signs and community entrance signs.
- Assumptions: Two (2) community workshops with each community, logo development.
 - Quantity: Four (4) logos
 - Estimated Budget: Cost included in the Citywide Section VII.C.1.
 - Exclusions: Processing

Community Entrances:

- Description: Community entrance scenes are located at key entrances as outlined in the Beautification Master Plan. These scenes will signal to motorists that they are entering a particular community and will include a fence, monument, a couple of trees and landscaping.

- Assumptions: The Saugus scenes would include a stone wall instead of a three-rail fence and there are four (4) locations identified for Saugus.
 - Quantity: Twenty-one (21) (four (4) with stone wall)
 - Estimated Budget: \$12,900/scene with rail fence;
\$18,900/scene with stone wall
 - Exclusions: Land acquisition, earth movement, site preparation, processing, design and environmental review.

3. Key Intersection Treatments:

- Description: Key intersections have been identified in each community for improvements. The improvements would include accent paving, landscaping and street trees, consolidation of signage (costs not included), widening medians where possible, restriping lanes where needed, and lighting and signage pole treatments. The locations of these intersections are outlined in the Beautification Master Plan on page I-23.
- Assumptions: Median expense included under median section; some intersections have room for landscaping and low walls and some do not; the existing light poles and traffic signal poles can be utilized with some added treatments. Intersections vary in size so there is a budget range listed below.
 - Quantity:
 - Canyon Country*: Four (4) primary intersections
 - Valencia*: Three (3) primary intersections (plus two (2) future primary), two (2) secondary
 - Newhall*: Two (2) primary intersections
 - Saugus*: Three (3) primary intersections (plus one (1) future intersection), three (3) secondary
 - Estimated Budget: \$86,000/smaller intersection;
\$191,000/larger intersection
 - Exclusions: Land acquisition, earth movement, site preparation, processing, design and environmental review.

E. Other Beautification Projects

1. Awards Programs, Banner Programs, and Public Art Programs:

- Description: These programs are encouraged for each community and may be pursued by private or non profit groups such as local community groups, art organizations, or the Chamber of Commerce.

2. Bridges (Signage and Art):

- Description: Introduce monument markers or City icon plaques at or on key pedestrian bridges throughout the City. These could be located where pedestrians or even vehicular traffic would see them as they approach the bridge.
- Assumptions: Not every location will need this improvement. This would be approximately 24" x 24" and made of ceramic tile with the possibly of art or graphics on the tile.
- Quantity: Unknown
- Estimated Budget: The budget would vary depending on artist. If a logo was simply applied to each tile, the costs would be as low as \$50 per tile.
- Exclusions: Preparation of graphics to apply to tile, design of tile, installation, processing.

3. Miscellaneous Projects:

These are projects that do not have estimated budgets, but should be considered when beautifying Santa Clarita.

- Santa Clara River should be exposed and improved and used as a unifying element through Santa Clarita (pedestrian and bikeway opportunity).
- More trails created throughout Santa Clarita (particularly in Newhall and Saugus).
- Continued effort to remove advertisement billboards.

Note: The previous statements of probable construction costs are for preliminary budgetary purposed only. Estimated unit prices are based on elements shown on the schematic plans only and are not based on detailed design plan or construction documents. This statement of probable construction cost does not account for unforeseen site conditions, land acquisition, earth movement, site preparation, processing, environmental reports, design, specifications and cost estimating.